Ravenna North

2nd Recommendation Meeting / Design Review
2101 NE 88th St
DPD #3023106

March 6, 2017 6:30PM

nk
NICHOLSON KOVALCHICK ARCHITECTS

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RAVENNA N: A contemporary, urban, lushly planted community

PROJECT TEAM:

Applicant: 23rd Ave NE Townhomes, LLC
419 Occidental Ave S
Seattle, WA 98104
Suite 300
Contact: David MacDuff

Architect: NK Architects
310 First Ave S.
Suite 4S
Seattle, WA 98104
Contact: Christine Goodwin

Landscape Architect: Weisman Design Group Inc
2329 E. Madison St
Seattle, WA 98112
Contact: Nick Hagan

SDCI Project #3023106
Contact: Carly Guillory

EXISTING SITE:

Address: 2101 NE 88th St.
Location: SE corner of Lake City Way NE and NE 88th St
Site Area: 157,639 Sq Ft (or 3.62 Acres)
Existing Development: 1 existing structure (recreation bld) and approximately 75 mobile homes.

PROJECT PROGRAM:

Number of Residential Units: 87
Number of Garage Parking Stalls: 115
Number of Guest parking stalls: 23
Area of Residential Use: Approximately 134,134 Sq Ft
Area of Garage Use: Approximately 29,018 Sq Ft
Total Area: Approximately 163,152 Sq Ft
Proposed FAR: 0.96

DEVELOPMENT OBJECTIVE:

To provide a community of attractive, owner-occupied townhomes that combine a clean and contemporary style with pitched roofs, active outdoor spaces, and results in a project that enhances the neighborhood while respecting its neighbors.

Index

Hierarchy of Open Space - Initial DR

BOARD FEEDBACK:

At our Initial DR Meeting on November 21, 2016, the Board gave us feedback concentrated feedback in the three areas listed below. We appreciated, and have responded to, the Board's direction. Detailed responses to each Board Comment can be found on pages 29-31.

1. PEDESTRIAN WALKWAYS AND CIRCULATION

The new site plan provides for increased safety as pedestrians travel to all parts of the site. Per the Board's suggestion, 13 designated concrete pedestrian crossings will be provided.

2. HIERARCHY OF OPEN SPACE

The new site plan has significantly more open space that the plan presented at the additional DR Meeting. This allows for a Hierarchy of Open Space and distinct usable green spaces. The most important change to the site plan was the Community Gathering Space moved to the NW corner. This move away from traffic of NE 88th St, provides for a larger, safer, more comfortable green space. This area along with the Entry Plaza comprise the project's Primary Green Spaces. The Entry Courts, located between buildings, serve as Secondary Green Spaces. Per feedback from the Board, large evergreen trees will be located mostly in tertiary green space to serves as "Urban Forests" and provide a buffer between the project and its neighbors.

3. ARCHITECTURAL CONCEPT

The project's "Townhouse Typology" has been defined and strengthened. Shed roof forms delineate individual units and vertical continuity has been strengthened through the use of fin walls and materiality.

OLD SITE PLAN FROM DR1 - HEIRARCHY OF OPEN SPACE

NEW SITE PLAN - HIERARCHY OF OPEN SPACE DIARGRAM

NEW PLAN HIGHLIGHTS:

- 13% INCREASE IN OPEN SPACE
- 7% INCREASE IN PLANTING AREA
- 44% INCREASE IN PLANTINGS ADJACENT OR DIRECTLY OPPOSITE PEDESTRIAN PATHS
- 30% INCREASE IN BUILDING SEPARATION AT ENTRY COURTS
- 3 FEWER BUILLDINGS, 2 FEWER UNITS TOTAL

PRIMARY PEDESTRIAN PATH
SECONDARY PEDESTRIAN PATH
VEHICLE TRAFFIC
PRIMARY GREEN SPACE
SECONDARY GREEN SPACE
TERTIARY GREEN SPACE
RAISED PEDESTRIAN CROSSING
CONCRETE PEDESTRIAN CROSSING

Response to Board

Hierarchy of Open Space - Proposed

A strong hierarchy of open space drives the new site plan.

Primary Green Spaces are the generous outdoor amenity areas intended to be used by all residents of the community. The Community Gathering Area will contain natural play features for children, as well as seating for adults. A winding path leads from the north to the south end of the space, running alongside a large grove of trees. Additional information can be found on pages 6-7 of this packet.

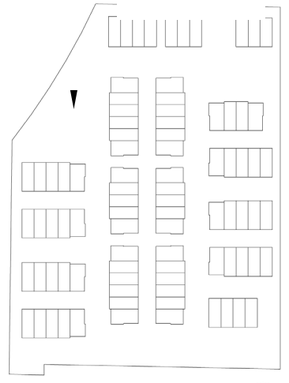
The Entry Plaza is now located in the NE corner of the site, just south of Building C. Guest parking is provided for visitors and for the ease of residents stopping to pick up their mail. This natural gathering space will be supplemented with outdoor seating and appropriate plantings, including plantings between the parking area and Building D to screen headlights. Additional information can be found on pages 8-9 of this packet.

Secondary Green Spaces are located at the Entry Courts between building front doors. These spaces are intended to be used and occupied, but not as widely as the Primary Green Spaces. Raised pedestrian crossings are provided at the entrance to all Primary and Secondary Green Spaces. Additional information can be found on pages 10-11 of this packet.

Tertiary Green Spaces are located throughout the project, but especially along the site perimeter. These areas are heavily planted, and will contribute to the look and feel of the community, but they are not intended to be occupied. These "Urban Forest" areas are primarily where evergreen trees will be located. Additional information can be found on pages 12-13 of this packet.

Hierarchy of Open Space - Community Gathering Area

NEW COMMUNITY GATERING AREA - RENDERING



UNIT LOCATION PLAN

NEW COMMUNITY GATHERING AREA - RENDERED SITE PLAN

Response to Board

Board Feedback:

- (DR1) The Board noted the large amount of paving on site, and reiterated the importance of distinguishing spaces and providing clear and safe pedestrian circulation.
- (DR1) Due to the size of the site, a greater distinction of open space was expected. The Board agreed the open space program was not compelling and lacked hierarchy. To instill a sense of hierarchy, consolidating the open space into a larger area was suggested. (DC3-B)

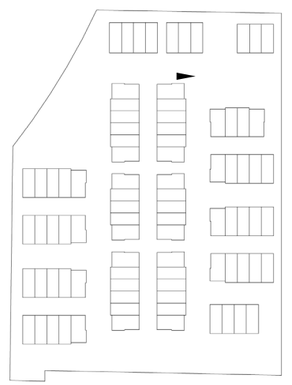
Design Response:

Locating the Community Gathering Area in the NW corner of the site provided opportunities to address the Board's feedback. Due to the site's location below LCW, this area is sheltered from the street and buffered by a large grove of existing trees. Green screens will be used to soften the adjacent retaining wall and contribute to the park feel. Locating the space away from the traffic of NE makes in safer for children and the move away from the entrance driveway provides a logical space for raised pedestrian crossing at the north and south. Concrete paving will be used in the area between the raised crossings in order to provide a visual clue to drivers to be alert.

OLD COMMUNITY GATHERING AREA PRESENTED AT DR1

Hierarchy of Open Space - Entry Plaza

NEW ENTRY PLAZA - RENDERING



UNIT LOCATION PLAN

NEW ENTRY PLAZA - RENDERED SITE PLAN

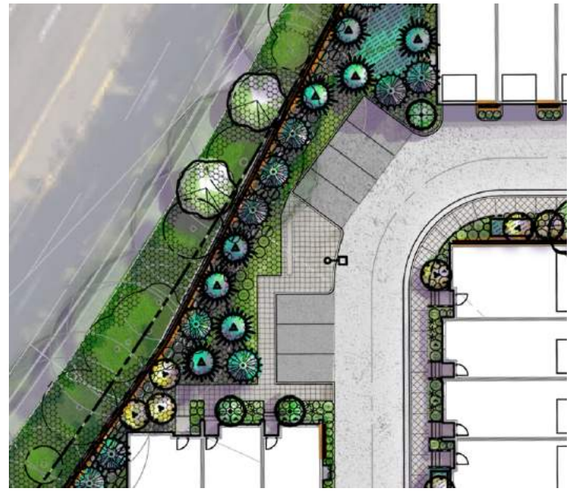
Response to Board

Board Feedback:

- (EDG) The Board recommended exploration of creating a "network" of greenspace throughout the site that result in usable open spaces (PL1-A, DC3-B).
- (DR1) The Board reiterated the importance of safe pedestrian walkways and circulation, and recommended the addition of traffic calming solutions for the central vehicular roadway, to slow traffic and improve the pedestrian experience (DC1-B).

Design Response:

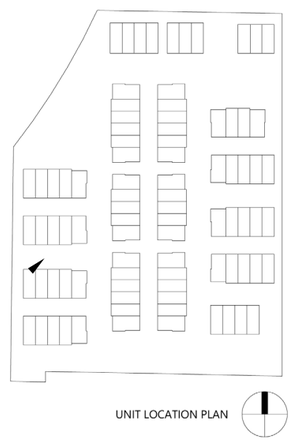
In order to better utilize green spaces, Building C was moved north to front NE 88th Street, as Buildings A & B do. This allowed for more usable green space adjacent to the mail area, as well as temporary and visitor parking right at the entrance to the Community. Wayfinding signage will be provided to orient new visitors. A raised pedestrian crossing connects the central loop sidewalk to picnic tables, bike racks, and the mail kiosk.

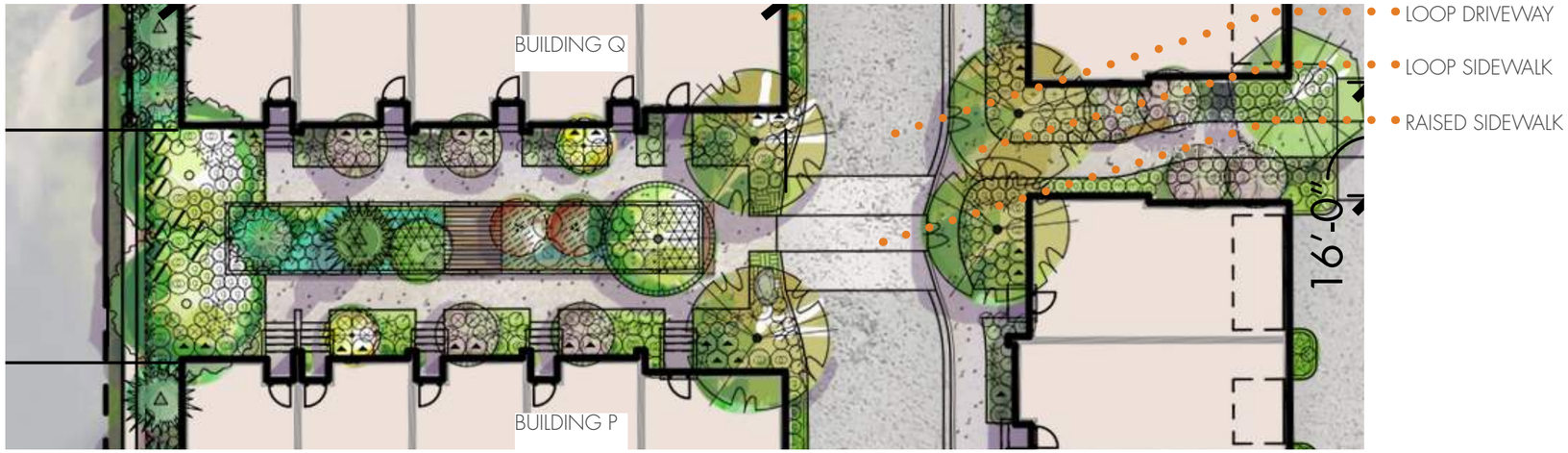


OLD MAIL AREA PRESENTED AT DR1

Hierarchy of Open Space - Entry Courts

TYPICAL ENTRY COURT - RENDERING





NEW TYPICAL ENTRY COURT - RENDERED SITE PLAN

NEW ENTRY COURT IDENTIFIERS

Response to Board

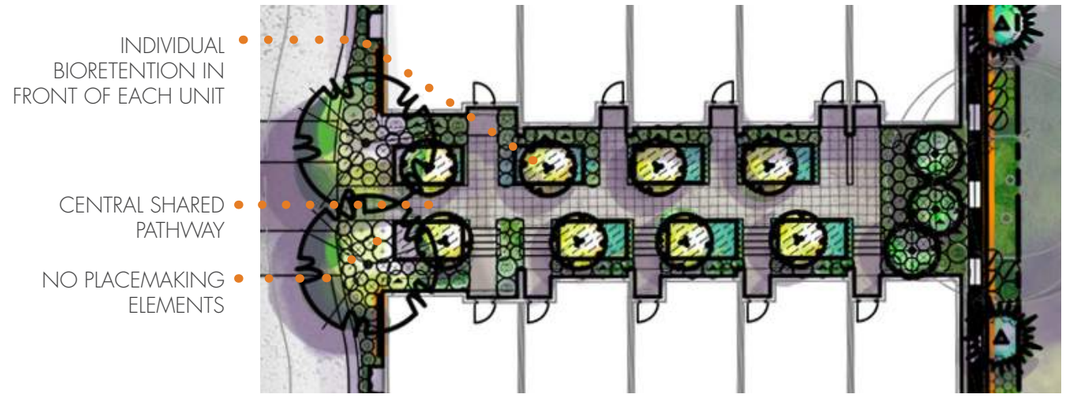
Board Feedback:

- (DR1) Due to the size of the site, a greater distinction of open space was expected. The Board agreed the open space program was not compelling and lacked hierarchy. To instill a sense of hierarchy, consolidating the open space into a larger area was suggested. (DC3-B)

Design Response:

As at the Initial DR Meeting, Entry Courts are provided where units face on another. These landscaped areas are designed to be used and occupied, but not as heavily as the Primary Green Spaces. As the site plan was refined after DR1, these Entry Courts got 30% wider, providing between 8' and 11' more separation between buildings. At-grade bioretention facilities are large enough for deciduous trees and other plantings, while still allowing for individual planters at each unit entry way which help to identify and distinguish each unit.

Lastly, although the overall concept of each Entry Court is the same, unique identifiers are planned at the point of entry for each court. These elements include a rock feature, a pergola, reclaimed wood, and unique tree specimens.



OLD ENTRY COURT LAYOUT PRESENTED AT DR1

Hierarchy of Open Space - Urban Forest

EXISTING EXCEPTIONAL TREES

EXISTING MULTIFAMILY APARTMENT ACROSS PROPERTY LINE

INFILL TREES AND PLANTS

12' LIGHT POLE

RAISED PEDESTRIAN CROSSWALK

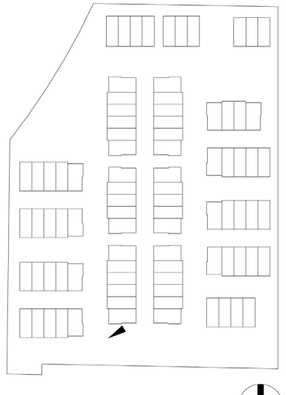
ENTRY COURT IDENTIFIER

GUEST PARKING ALONG SOUTH PROPERTY LINE

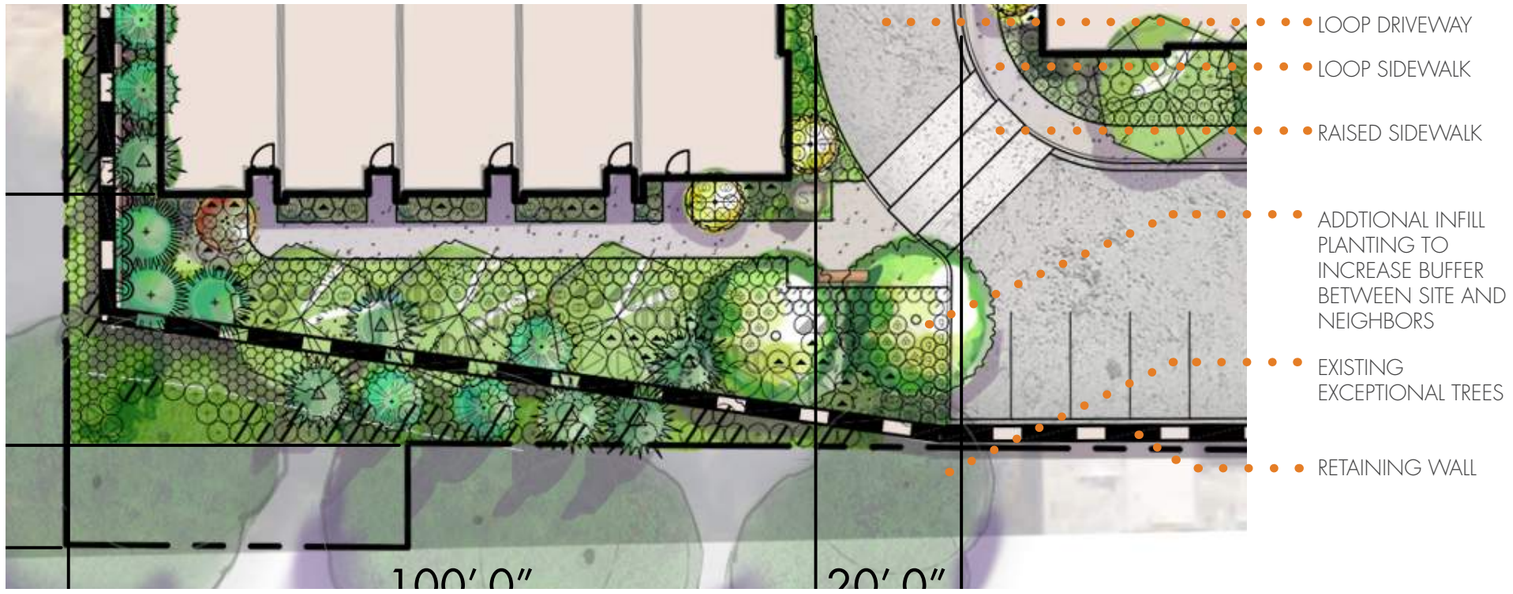
SIDEWALK LOOP

DRIVE AISLE LOOP

NEW AND EXISTING TREES - RENDERING



UNIT LOCATION PLAN



NEW RENDERED SITE PLAN AT URBAN FOREST

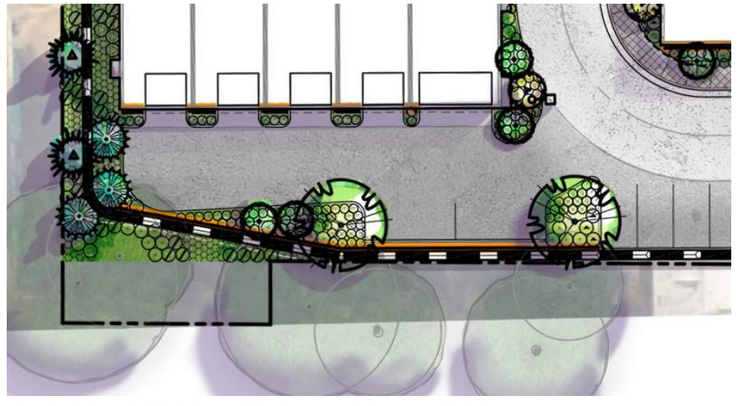
Response to Board

Board Feedback:

- (DR1) Due to the size of the site, a greater distinction of open space was expected. The Board agreed the open space program was not compelling and lacked hierarchy. To instill a sense of hierarchy, consolidating the open space into a larger area was suggested. (DC3-B)

Design Response:

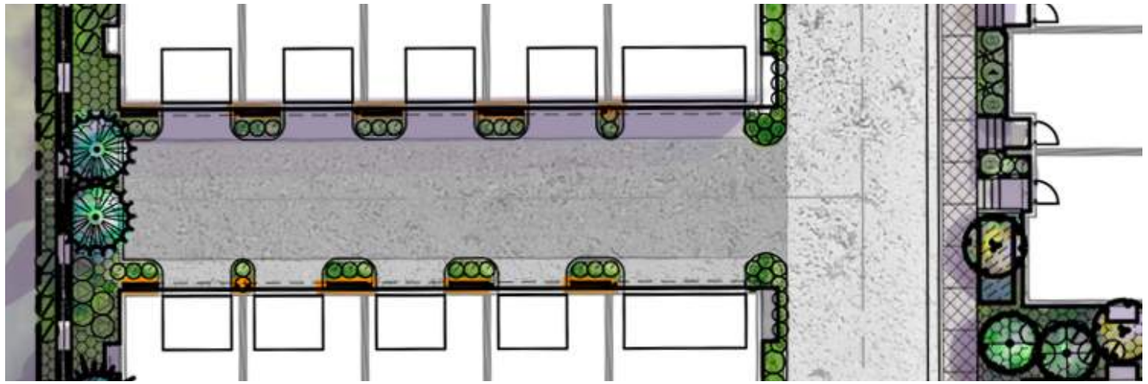
Tree selection and location has been carefully considered by the design team. During the first Recommendation meeting, the Board asked evergreen trees be used sparingly or not at all in areas intended for active use. In the Primary and Secondary Green Spaces, deciduous and small evergreen trees are used, so that people recognize these spaces as inviting. In the Tertiary Green Spaces, the project utilizes evergreen trees in order to provide landscape buffers and to create "urban forest" areas. These are not intended to be actively used, but to create lush landscaping at the project's edge. In many cases, these infill trees work with existing Exceptional perimeter trees and groves. They also provide privacy, both for this project and its neighbors.



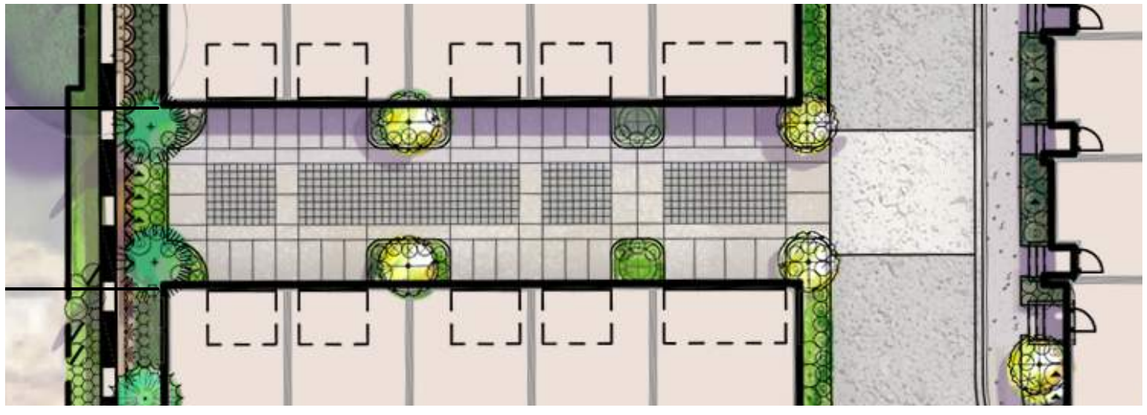
OLD RENDERED SITE PLAN FROM DR1

Hierarchy of Open Space - Motor Courts

NEW TYPICAL MOTOR COURT - RENDERING



OLD RENDERED STIE PLAN AT DR1 - MOTOR COURTS



NEW RENDERED SITE PLAN - MOTOR COURTS

Response to Board

Board Feedback:

- (DR1) The Board agreed there is an opportunity for the auto courts to be designed such that they are viewed as back patios for the residents, and recommended further development of these areas. The introduction of planters, trees, and variety of hardscape patterns was suggested. (DC3-B)

Design Response:

In order to ensure that the motor courts function as residential back patios, several changes were made from the initial DR Meeting. Primarily, a scored concrete pattern will be used to signal to drivers that this is spaced is shared between people and cars. Juliette balconies facing down into the motor courts were added to the interior units in order to provide more interaction between interior and exterior space. Additional upper level glazing was added to provide additional eyes on these areas. Glazing was added to the garage doors. Larger planters break up the expanse of paving and introduce small trees into these areas. Garage doors can be opened up allow activity to spill outside and further increase the sense that these spaces act as “back patios” for the residents.

Pedestrian Walkways & Circulation

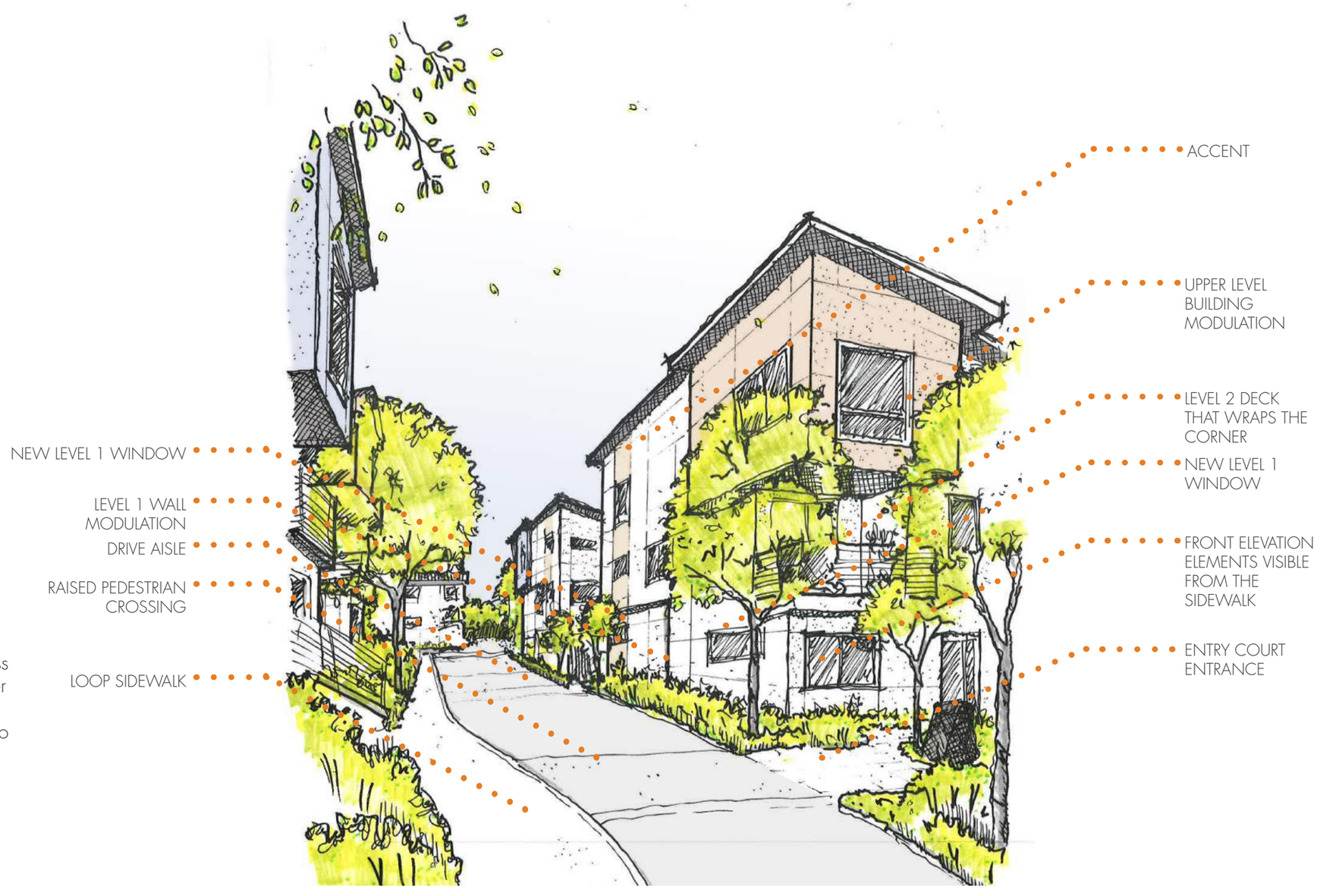
Board Feedback:

- (DR1) The Board recommended the introduction of windows and plantings to mitigate the blank wall condition while respecting safety and security concerns of the unit residents. (DC2-B)
- (DR1) The Board noted that the change in materials and color application is not easily understandable. The Board suggested using reveal patterns and detailing to articulate changes in material. A change in material without a change in plane is not supported. (DC2-B)

Design Response:

At the direction of the board, the team explored how to create more active side elevations for the perimeter units that face the drive aisle. At Level 1, an additional high window was added in the garage.

Additional building modulation is now proposed by insetting the Level 1 wall at the garage. This allows for shadow lines as well as increasing the width of the planter. Since the central sidewalk is generally not adjacent to the perimeter buildings, these walls are generally experienced from across the drive aisle. Because of this, more of the upper levels are visible and the wrap around deck, glazing, and upper level building modulation also contributes to the experience of these facades.



NEW PEDESTRIAN EXPERIENCE SKETCH - VIEW FROM LOOP SIDEWALK

1. Scored concrete (motor courts and drive aisle crossings)

2. Raised concrete pedestrian crossings

3. Asphalt drive aisle

4. Permeable concrete sidewalks

Response to Board

Board Feedback:

- (DR1) The Board recommended the use of hardscape materials to create more visual distinction of spaces such as the auto courts, roadway, and pedestrian walkways. Permeable asphalt should not be used in the auto courts. (DC3-B, DC4-D)
- (DR1) The Board reiterated the importance of safe pedestrian walkways and circulation, and recommended the addition of traffic calming solutions for the central vehicular roadway, to slow traffic and improve the pedestrian experience (DC1-B).
- One internal sidewalk loop is proposed, with crossings at the center of the site at the central landscape spine. The Board expressed concern that the pedestrian walkways and circulation are not clearly identified or adequately provided. The Board recommended using a variety of hardscape to differentiate pedestrian walkways from the central vehicular drive.

Design Response:

Hardscape materials are used to differentiate between spaces. Permeable concrete sidewalks and paths intended for pedestrians are visually distinguishable from the asphalt drive aisle and provide pedestrian access to all areas of the site. In locations where pedestrians are encouraged to cross the drive aisle, either raised concrete or scored concrete connections will be provided. Where pedestrian paths cross the central auto-alley, the alley narrows significantly. Scored concrete motor courts can be utilized for spontaneous play. In addition, the section of the drive aisle adjacent to the Community Gathering Area will be scored concrete, signaling to drivers that they should slow down.

The green spaces adjacent to interior sidewalks will be appropriately landscaped.

Response to Board

New Site Plan

THEMES FOUND THROUGHOUT SITE:

A network of functional greenspaces and pedestrian pathways connect to create a clear site hierarchy

Where pedestrians need to cross the drive aisle to order to get to units or shared green spaces, pedestrian crossings are provided and delineated with a change of material

Increased building separation at entry courts allows for greater flexibility with planting, including at-grade bioretention and deciduous trees.

Place-making landscape elements have been incorporated at each entry court and aid in site wayfinding.

Benches are scattered throughout the site and within each primary green space and entry court.

Human-scaled paving patterns at motor courts encourage and allow for spontaneous play.

SIDE (WEST) ELEVATION - PUBLIC EDGE - ADJACENT SIDEWALK

TOWNHOUSE FRONT FACADE - RENDERING

Response to Board

Entry Sequence

Most visitors will arrive at the site via or adjacent to the entry driveway off of NE 88th. A small running between Buildings A & B will allow more direct pedestrian access for people coming from Lake City Way. Because of this, careful consideration was given to Buildings B, C, and K, which will be the first that are encountered.

While all three of these buildings utilize shed roofs, each has a unique roofline, which provides variety to the project.

Since the main pedestrian entrance to the site is adjacent to Building C, careful consideration was given to the human experience there. The front porch and the Level 2 deck are both open to NE 88th St and the entry sidewalk. Level 1 also steps back from the level above in order to provide relief. Additional glazing has been added to all levels of the facade.

Additional windows have been added to the east facade of Building B and the steep slope adjacent to Levels 1 & 2 will be heavily landscaped.

Response to Board

Architectural Concept - Townhouse Typology

Board Feedback:

- The Board directed further exploration of the townhouse typology and introduction of elements and design solutions to enhance the individual character of each unit. (DC2-B)
- The second and third floors lack cohesion while the ground level reads as a large plinth of cement siding. (DC2-B)
- The Board noted that the siding and the roof forms appear very independent, leaving much opportunity for the siding to engage the roof. A stronger dialogue between the siding and roof form is needed. (DC2-B)

Design Response:

How do we define Townhouse Typology?

Our townhouse typology has three distinct elements:

- Vertical continuity of individual units from ground to roof
- Individualized entries, with stoops where possible
- Human-scale building modulation

Strengthening the Townhouse Typology lead to key changes to the building elevations. Shed roofs were added to each building in order to delineate individual units. As requested by the Board, the roof forms work with the siding and accent locations. Additional information on the variety of shed rooflines can be found on the following pages.

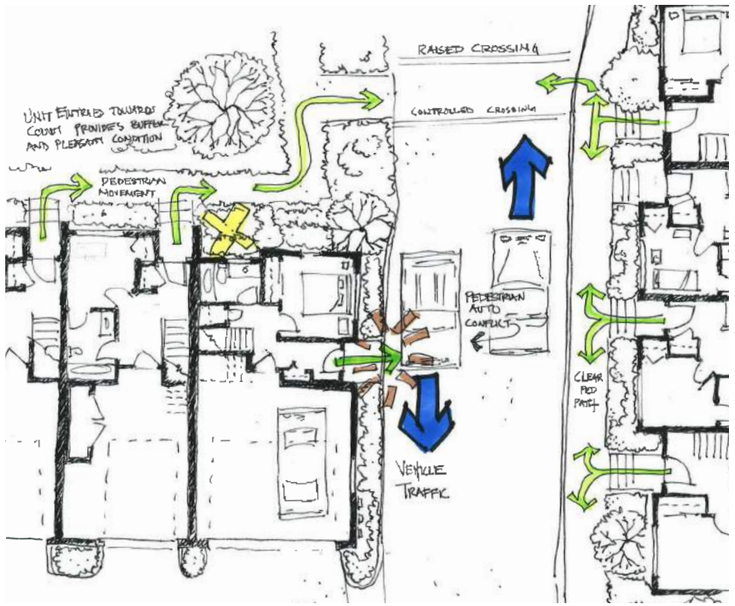
The vertical fin walls used in the first DR designed were retained. These were used to reinforce the material and color accents which now extend down to the second level and increase cohesion between floor levels. About half of the units have small unit entry stoops. Buildings A & B have bolt-on decks oriented to the interior of the site, while the other units have large second level decks along the front elevation.

OLD FRONT ELEVATION PRESENTED AT DR1 MEETING

NEW FRONT ELEVATION (BUILDING P SHOWN)

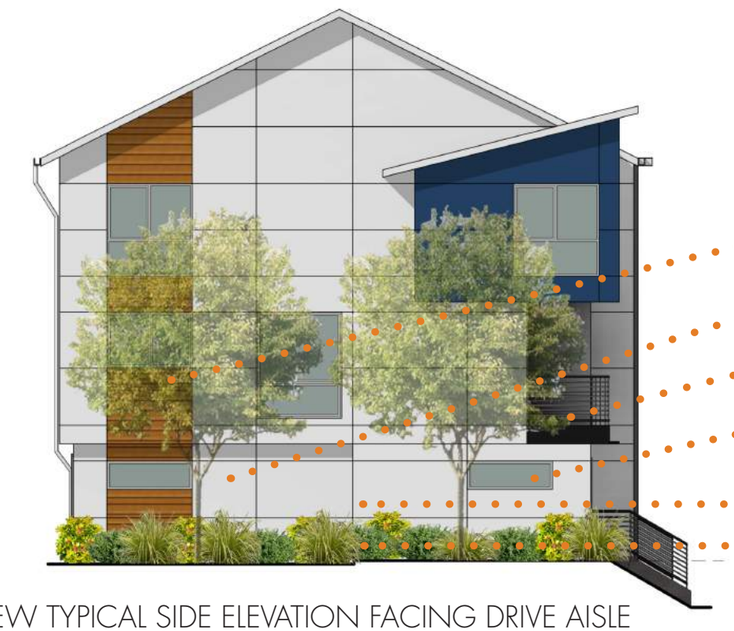

BUILDING Q
ENTRY STOOPS
PATHS TO UNIT ENTRY DOORS
FIN WALL EXTENDS DOWN TO LEVEL 1
SHED ROOFS AND FIN WALLS DELINEATING INDIVIDUAL TOWNHOMES
ALLEY-LOADED BUILDINGS BEYOND
LOOP SIDEWALK
RAISED PEDESTRIAN CROSSING

Architectural Concept - Townhouse Typology



STUDY - SIDE ENTRANCE

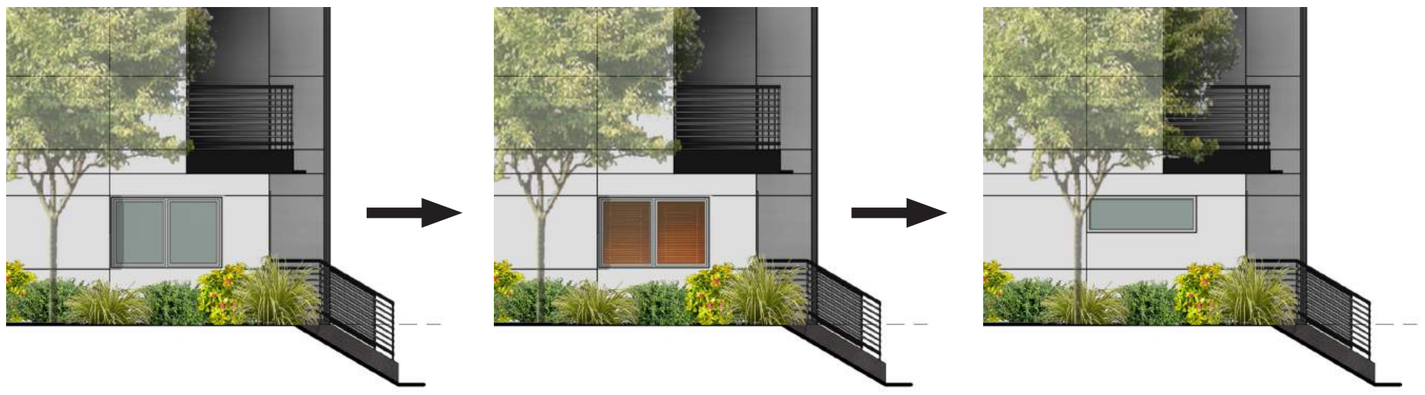
OLD TYPICAL SIDE ELEVATION FACING DRIVE AISLE



- ACCENT EXTENDS DOWN TO THE GROUND
- "PLINTH" AT LEVEL 1 HAS BEEN ELIMINATED
- WRAP-AROUND LEVEL 2 DECKS PROVIDE FOR ADDITIONAL EYES ON THE SIDEWALK AND DRIVE AISLE
- NEW HIGH BEDROOM WINDOW
- PLANTING - TREES AS WELL AS LOW-LEVEL SHRUBS
- WALL MODULATION

NEW TYPICAL SIDE ELEVATION FACING DRIVE AISLE
NOTE: SEE PAGE 16 FOR SKETCH OF TYP. PEDESTRIAN VIEW OF THIS FACADE FROM THE LOOP SIDEWALK

RENDERING - SIDE ELEVATION AND MOTOR COURT



STUDY - SIDE BEDROOM WINDOW

Response to Board

Board Feedback:

- (DR1) The Board recommended the introduction of windows and plantings to mitigate the blank wall condition while respecting safety and security concerns of the unit residents. (DC2-B)
- (DR1) The Board noted that the change in materials and color application is not easily understandable. The Board suggested using reveal patterns and detailing to articulate changes in material. A change in material without a change in plane is not supported. (DC2-B)

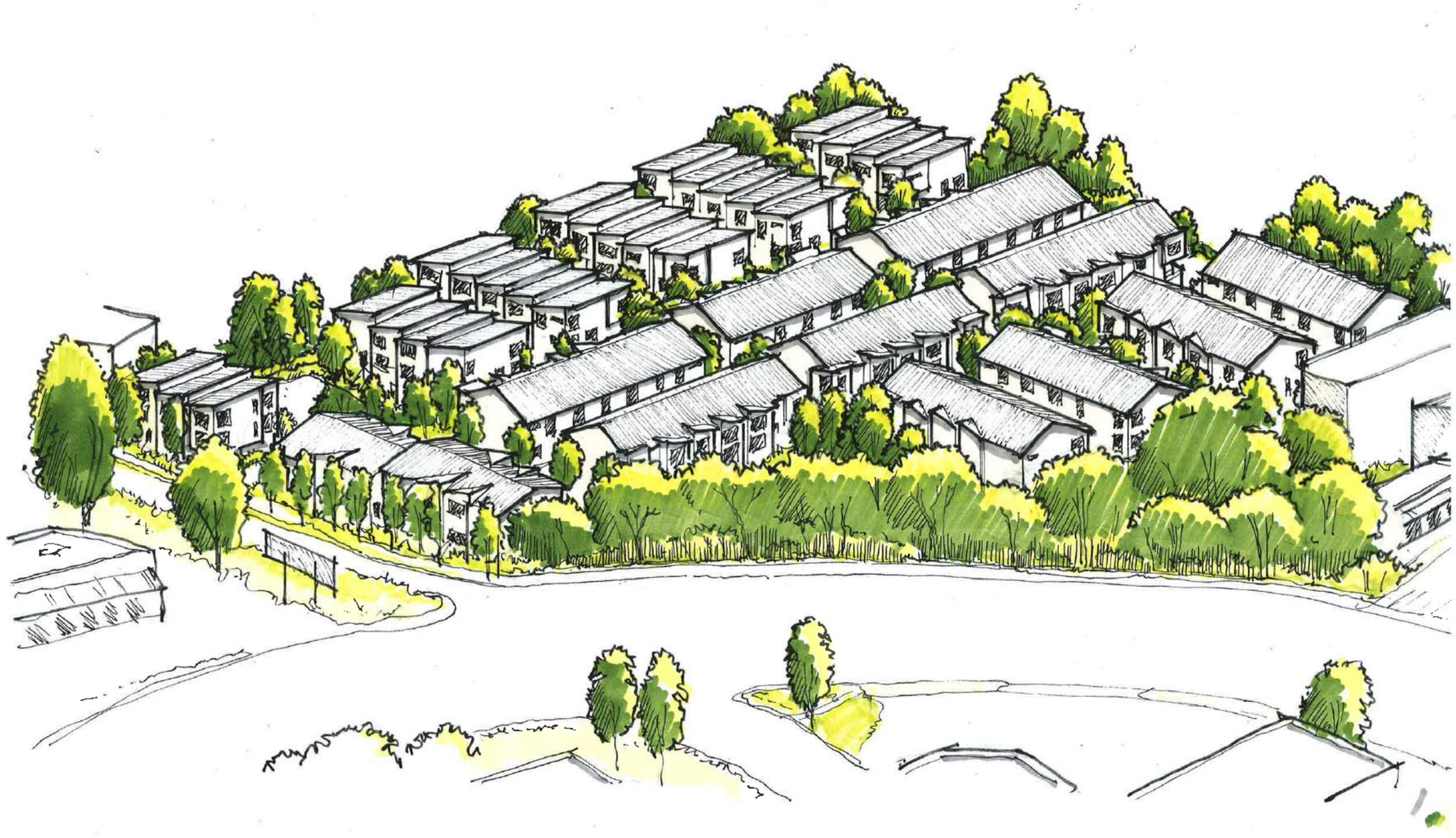
Design Response:

At the direction of the board, the team explored how to create more active side elevations for the perimeter units that face the drive aisle. At Level 1, an additional high window was added in the garage. A larger bedroom window was considered, but it was concluded a larger bedroom window would be more likely have the shades constantly drawn for privacy, limiting the benefit of the new window. Therefore, the project is proposing a high window.

Additional building modulation is now proposed by insetting the Level 1 wall at the garage. This allows for shadow lines as well as increasing the width of the planter. Since the central sidewalk is generally not adjacent to the perimeter buildings, these walls are generally experienced from across the drive aisle. Because of this, more of the upper levels are visible and the wrap around deck, glazing, and upper level building modulation also contributes to the experience of these facades.

Another option we explored was facing the front door of the end unit towards the drive aisle. This scheme had several cons. From a resident perspective, it meant losing the gracious entry facing out on to the landscaped entry court. From a safety perspective, it meant pedestrian leaving the building at a point without an adjacent sidewalk or a raised crossing. In addition, it meant that we'd lose the vertical connection from the fin wall adjacent to the door up to the shed roof.

Architectural Concept - Roofline Variety



BIRD'S EYE VIEW OF VARIOUS ROOF LINES

BUILDINGS A & B - FRONT ELEVATION

FRONT ELEVATION - SHED ROOF (BUILDING F SHOWN)
USED ON BUILDINGS C, D, E, F, G, H

Response to Board

Architectural Concept - Roofline Variety

Because it was important to the design team that the community retained some of the gable roof forms that the neighbors were so supportive of a mix of gable and shed roof forms is proposed. On buildings where gable roofs are proposed as the dominant roof forms, small shed roofs have been added in order to strengthen the Townhouse Typology.

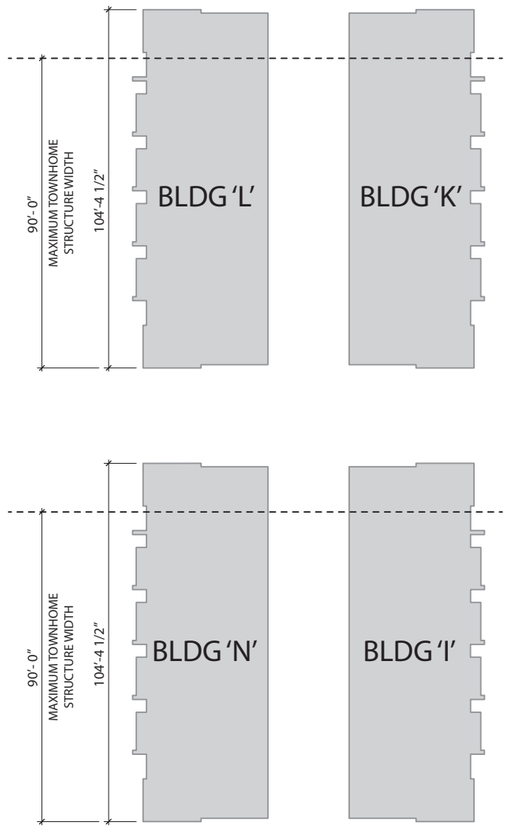
The buildings along the west property line will be among the most visible in the project, as they'll be seen not only by properties that are directly adjacent, but by the much greater number of community members who utilize Lake City Way. Because of this, we are proposing the neighbor-supported gable roof form for these buildings. Using the gable roof form for the alley-loaded buildings as well meant that the full variety of rooflines will greet visitors on their entrance to the project. Buildings A & B have a similar gable and shed combination.

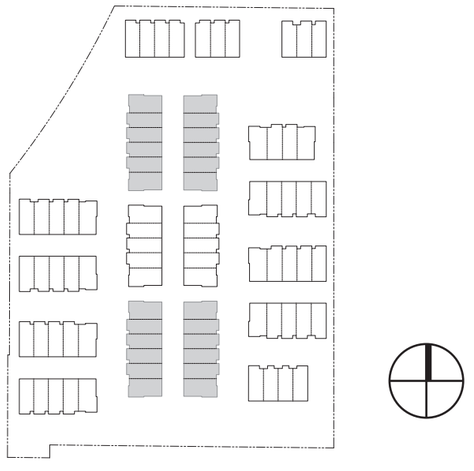
The six buildings utilize shed roofs only. This more contemporary look complements our other elevations in the project and provides variety. Because the roofs slope down towards the neighbors, from adjacent properties, they will read as clean and simple rooflines, much like gables.

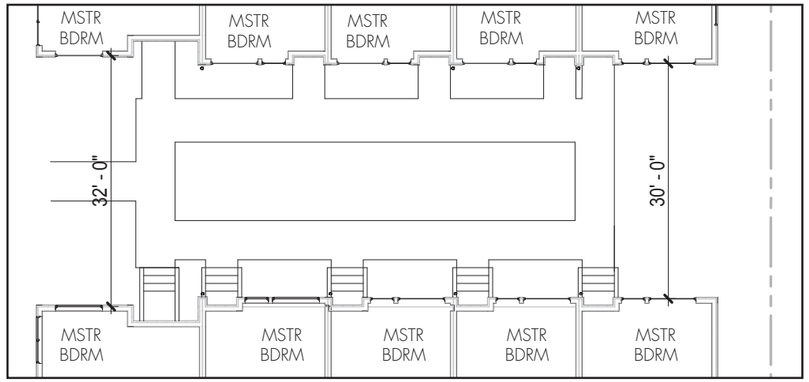
FRONT ELEVATION - GABLE ROOF (BUILDING J SHOWN)
USED ON BUILDINGS I, J, K, L, M, N, O, P, Q, R

Departure Diagram

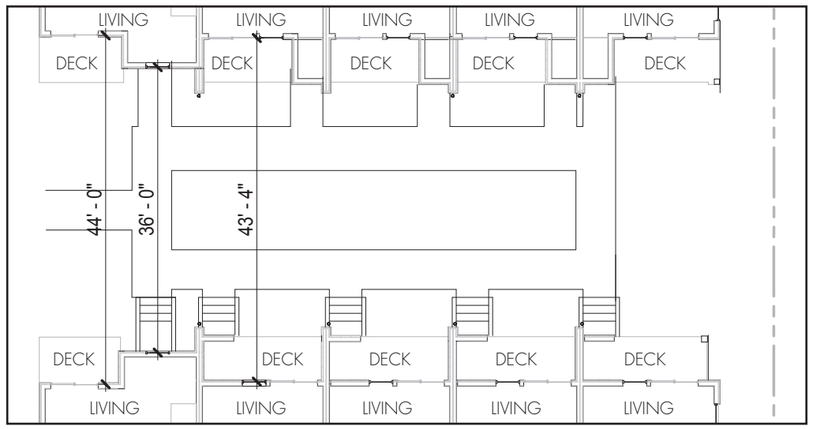
DEVELOPMENT STANDARD	REQUIREMENT	PROPOSED	DEPARTURE AMOUNT	REASON FOR DEPARTURE	DESIGN REVIEW GUIDELINES
SMC 23.45.527.A	STRUCTURE WIDTH IN LR-2 ZONES MAY NOT EXCEED 90'	104'-4 1/2"	AN ADDITIONAL 14'-4 1/2"	BOARD RECOMMENDED CLUSTERING ALLEY-LOADED UNITS TOGETHER IN ORDER TO INCREASE THE GREEN SPACE BE-TWEEN BUILDNGS	PL1-A-1, PL1-A-2, PL1-B1



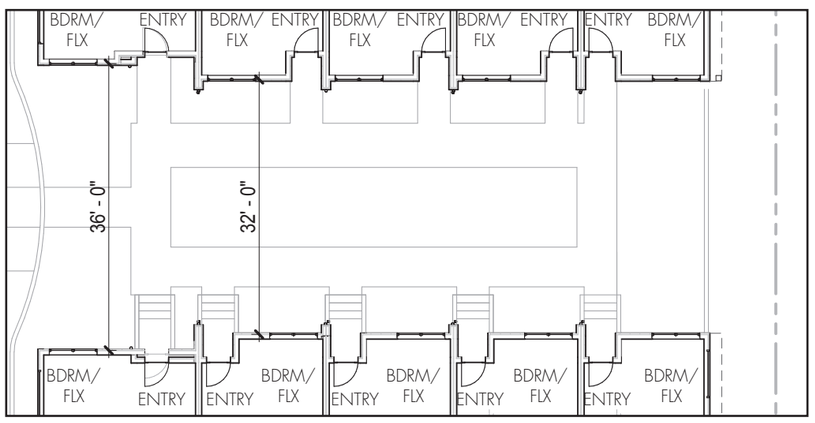




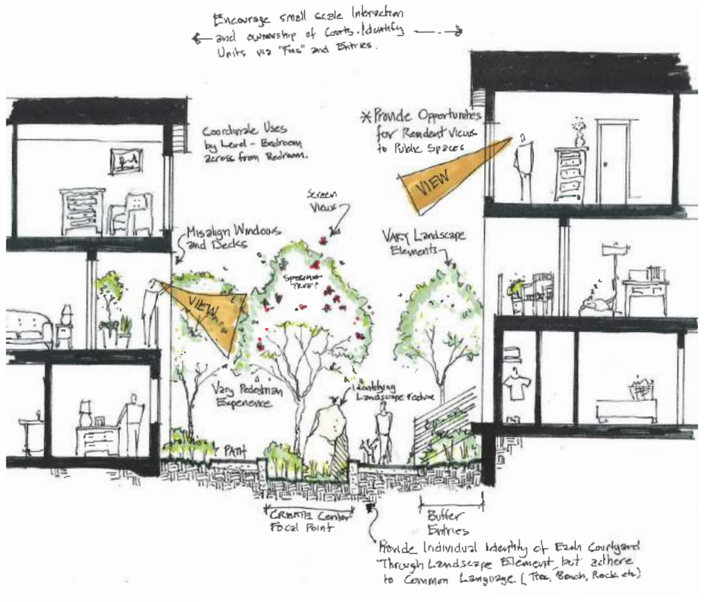
BUILDING SEPARATION AT LEVEL 3 IN ENTRY COURTS, TYP



BUILDING SEPARATION AT LEVEL 2 IN ENTRY COURTS, TYP



BUILDING SEPARATION AT LEVEL 1 IN ENTRY COURTS, TYP



NEW ENTRY COURT CONCEPT SKETCH

GLAZING OVERLAP BETWEEN BUILDINGS AT ENTRY COURT (OVERLAP SHOWN IN BLUE)

Response to Board

Privacy Diagrams

As requested, typical privacy floor plans and a diagram has been provided.

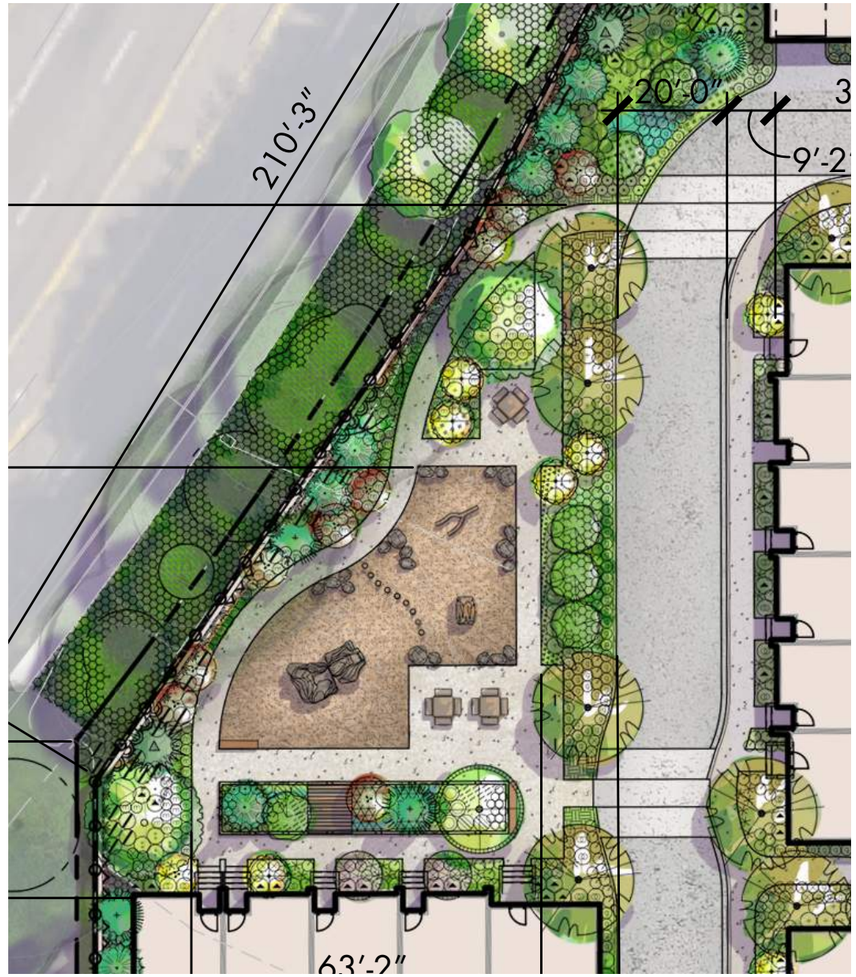
There is some overlap between glazing in units that face each other across the Entry Courts. This is mitigated by several circumstances. First, these facades are at a minimum 30' apart, which is 27-34% more building separation over the layout proposed at DR1. Second, due do the grading of the site, units that face each other are not at the same finish floor level and this difference means that the eye level of the units are not directly across from each other.

At different levels, there are additional considerations. Interior unit plans were deliberately mirrored so that front doors are offset where possible and planting provides some screening for the first and second levels. In addition, the Level 2 deck means that the interior of the unit is further recessed.

Response to Board

At the suggestion of the Land Use Planner, a design that uses woonerf principles was considered for the Community Gathering Area. This would result in a significant reduction in planting area, and increased safety concerns. The team felt it was unwise to blur the distinction between road and pedestrian space in an area intended for children. The final design proposal results in a heavily landscaped, tree-lined street, with designated pedestrian crossings and a clear delineation between car and pedestrian traffic.

DESIGN DEVELOPMENT CONSIDERATION - WOONERF PRINCIPLES



PROPOSED NEW DESIGN @ GATHERING AREA

FIRST RECOMMENDATION MEETING

1. PEDESTRIAN WALKWAYS AND CIRCULATION

The Board noted the large amount of pavement on site, and reiterated the importance of distinguishing spaces and providing clear and safe pedestrian circulation.

Current Proposal:

- 13% increase in open space
- 7% increase in planting area
- Differentiation between pavement types for different uses serves as a visual cue to homeowners and visitors
- 11 additional pedestrian crossings
- See specific responses to 1a - 1d below
- Additional information and graphics can be found on page 17

1a. Board Direction:

One internal sidewalk loop is proposed, with crossings at the center of the site at the central landscape spine. The Board expressed concern that the pedestrian walkways and circulation are not clearly identified or adequately provided. The Board recommended using a variety of hardscape to differentiate pedestrian walkways from the central vehicular drive.

Applicable Guidelines: PL1-B, DC4-D

1a. Response:

Clear and safe pedestrian connections are provided to all parts of the site. Permeable concrete sidewalks and paths are visually distinguishable from the asphalt drive aisle. In locations where pedestrians are encouraged to cross the drive aisle, either raised concrete or scored concrete connections will be provided. Scored concrete motor courts can be utilized for spontaneous play. In addition, the section of the drive aisle adjacent to the Community Gathering Area will be scored concrete, signaling to drivers that they should slow down.

1b. Board Direction:

The proposal includes two pedestrian crossings from the central sidewalk across the vehicle roadway. The Board recommended adding more pedestrian crossings with a change in hardscape material.

Applicable Guidelines: PL1-B

1b. Response:

At primary pedestrian paths, where a pedestrian on the internal sidewalk loop must cross the drive aisle to reach a Primary or Secondary Green Space, a raised concrete pedestrian crossing will occur. At secondary pedestrian paths, the drive aisle surface will change from asphalt to concrete. In the two locations where pedestrians will cross the central alley, the alley narrows considerably, giving drivers a clear visual clue to slow down.

1c. Board Direction:

The Board recommended the addition of a pedestrian crossing from the central sidewalk to the shared play space at the northeast corner of the site.

Applicable Guidelines: PL1-B

1c. Response:

The site plan has been reconfigured in order to provide a more generous green area and locate the Play Area within the Community Gathering Space. This change meant that the Play Area was relocated from the northeast corner to the northwest corner of the site. This location is further away from the traffic of NE 88th St and substantially below the adjacent Lake City Way. There are raised sidewalk crossings both to the north and south, so that children have easy and safe access.

1d. Board Direction:

The Board reiterated the importance of safe pedestrian walkways and circulation, and recommended the addition of traffic calming solutions for the central vehicular roadway, to slow traffic and improve the pedestrian experience.

Applicable Guidelines: DC1-B

1d. Response:

Seven raised pedestrian crossings are proposed along the vehicular roadway. These will be located where primary pedestrian paths cross the drive aisle. Four of these crossings lead directly to the unit entry courts and unit front doors. The other three are located near Primary Green Spaces. One leads to the Mail Plaza , while the Community Gathering Area has two: one to the north and one to the south in order to slow traffic coming from both directions.

2. HIERARCHY OF OPEN SPACE

The Board expressed disappointment that the site plan and landscape concept had not developed substantially since the EDG phase. The importance of a hierarchy of open space was reiterated.

Current Proposal:

- 3 buildings and 2 units have been eliminated, allowing for the reconfiguration of the site plan
- A clear hierarchy of open space has been developed

Response to Board

- See specific responses to items 2a - 2d below
- Additional information and graphics can be found on pages 4-14

2a. Board Direction:

Due to the size of the site, a greater distinction of open space was expected. The Board agreed the open space program was not compelling and lacked hierarchy. To instill a sense of hierarchy, consolidating the open space into a larger area was suggested.

Applicable Guidelines: DC3-B

2a. Response:

The site plan has been adjusted in order to establish a better hierarchy of open spaces. Three buildings and two units were eliminated in order to provide additional green space. Two large, Primary Green Spaces are provided. The Community Gathering Space, which includes the Play Area increased in size by 55%. The mail kiosks were relocated to the northeast corner at the Entry Plaza, behind Building C. The mail kiosk is a natural hub of the site and patio seating allows people to gather. The area is generously landscaped, both to enliven the space and to provide privacy for the residents of Building D.

2b. Board Direction:

The Board recommended the use of hardscape materials to create more visual distinction of spaces such as auto courts, roadway, and pedestrian walkways. Permeable asphalt should not be used in the auto courts.

Applicable Guidelines: DC3-B, DC4-D

2b. Response:

Visually distinct paving has been provided. Asphalt is the proposed only in areas that are primarily for cars; the drive aisle, auto-alley, and in the parking area at the south end of the site. Concrete is used in pedestrian areas. Sidewalks will be permeable concrete. Where people will need to cross the drive aisle on foot, raised or scored concrete is proposed. Scored concrete is also proposed in the parking area south of Building C, in the drive aisle adjacent to the Community Gathering Space and in the motor courts. The propsed pattern for the motor courts can be seen in the rendering on page 14.

2c. Board Direction:

The Board agreed there is an opportunity for the auto courts to be designed such that they are viewed as back patios for the residents, and recommended further development of these areas. The introduction of planters, trees, and variety of hardscape patterns was suggested.

Applicable Guideline: DC3-B

2c. Response:

Juliette balconies have been added to the second level of the interior units, allowing for additional eyes on the motor courts and for them to function more like back patios. Small trees will be provided in larger planters and a paving pattern will be implemented that helps to reduce the visual impact of these motor courts to a more pedestrian scale. See page 14.

2d. Board Direction:

Include in the Recommendation packet greater detail describing the open spaces. Include perspectives.

Applicable Guidelines: DC3-B.

2d. Response:

Additional renderings have been provided.

See pages 6-7 for plans and renderings of the Community Gathering Space. See pages 8-9 for plans and renderings of the Entry Plaza. See pages 10-11 for plans and renderings of a typical Entry Court. See pages 12-13 for plans and renderings of an Urban Forest.

3. ARCHITECTURAL CONCEPT

Current Proposal

- See specific responses to items 3a - 3g below
- See addition information on pages 18-25

3a. Board Direction:

The Board recognized public comment that supported the gable roof form.

Applicable Guidelines: DC2-B

3a. Response:

A mix of gable and shed roof forms is proposed. On buildings where gable roofs are proposed as the dominant roof forms, small shed roofs have been added in order to strengthen the Townhouse Typology. In addition, in order to provide variety within the project, six buildings utilize shed roofs only. Recognizing that the neighbors are supportive of gable roofs as the dominant roof form, the buildings with gable roofs have been located along the west property line. These buildings will be among the most visible in the project, as they'll be seen not only by properties that are directly adjacent, but by the much greater number of community members who utilize Lake City Way. Using the gable roof form for the alley-loaded buildings, meant that the full variety of rooflines will greet visitors on their entrance to the project. It also means that the project can achieve a more balanced cut-fill site, as these shed roof buildings could have higher finish floors than a gable roof on the

same building would. See page 21 for additional information.

3b. Board Direction:

The lack of townhouse typology was discussed. The Board agreed the individual townhouse units lacked an individual expression, with the buildings reading as apartment structures. The Board directed further exploration of the townhouse typology and introduction of elements and design solutions to enhance the individual character of each unit.

Applicable Guidelines: DC2-B

3b. Response:

Our townhouse typology has three distinct elements:

- Vertical continuity of individual units from ground to roof
- Individualized entries, with stoops where possible
- Human-scale building modulation

Vertical fin walls extend down to the ground, strengthening the visual distinction between individual units. Accent colors and materials are continuous between levels 2 and 3. In addition, shed roofs have been added above most units, strengthening the townhouse typology. The gray lap siding has been eliminated at the ground level so that the buildings no longer appear to sit on a plinth. See pages 19-21.

3c. Board Direction:

The Board noted that the composition appears difficult to understand and lacks deliberate intent. The second and third floors lack cohesion while the ground level reads as a large plinth of cement siding.

Applicable Guidelines: DC2-B

3c. Response:

The ground level plinth has been eliminated and vertical elements extend to the ground.

3d. Board Direction:

The Board agreed the plinth is a blank horizontal expression, and not a successful response to Board guidance or Design Guidelines. This blank wall condition at the ground level was not supported, and the Board recommended the introduction of windows and plantings to mitigate the blank wall condition while respecting safety and security concerns of the residents.

Applicable Guidelines: DC2-B

3d. Response:

Where the side facades of perimeter units face the drive aisle, a number of responses have been utilized in order to avoid a blank walls condition. See diagram on page 22. Two long, high windows are provided, one at each end of the facade. In addition, building modulation at level 1 allows for additional planting space. The plinth and green screens have been eliminated.

3e. Board Direction:

The Board noted that the siding and roof forms appear very independent, leaving much opportunity for the siding to engage the roof. A stronger dialogue between the siding and roof form is needed.

Applicable Guidelines: DC2-B

3e. Response:

Roof forms have been revised to better engage with the siding and to express unit individuality. Accent colors and materials are located so that they relate to both roof forms and building modulation. See pages 20, 21, & 25.

3f. Board Direction:

The Board noted that the change in color and materials is not easily understandable. The Board suggested using reveal patterns and detailing to articulate changes in material. A change in material with a change in plane is not supported.

Applicable Guidelines: DC2-B

3f. Response:

Building modulation has been adjusted to minimize in-plane changes in materials. Where they occur, an appropriate reveal detail will be used. See pages 19-23.

3g. Board Direction:

The Board requested additional information illustrating window placement, and how each responds to abutting structures.

Applicable Guidelines: PL2-B, DC2-B

3g. Response:

Privacy diagrams have been added to the packet. See page 27.

Tree Replacement Plan

At the EDG meeting the board was in favor of removing exceptional trees internal to the site. Perimeter trees are intended to remain. Replacement trees are located throughout the site, as shown.

SDOT took the stance that they will not allow a pedestrian connection from the site up to the Lake City Way sidewalk. This is due to the fact that exceptional ROW trees run along the entire property line adjacent to LCW. Per SDOT, these trees are to remain.

LEGEND

SYMBOL	QTY.	DESCRIPTION
		DRIPLINE OF R.O.W. TREES
		CRITICAL ROOT ZONE OF R.O.W. TREES
		DRIPLINE OF ONSITE TREES TO REMAIN AND PROTECT
		DRIPLINE OF EXCEPTIONAL ONSITE TREES TO BE REMOVED
		EXISTING TREES:
	28	TREES TO REMAIN AND BE PROTECTED IN ACCORDANCE WITH CITY OF SEATTLE STANDARD PLAN NO. 132a, 132b, AND 133, LATEST EDITIONS.
	19	EXCEPTIONAL TREES (OR 24"+ DBH) TO BE REMOVED
	51	NON-EXCEPTIONAL TREES TO BE REMOVED
		REPLACEMENT TREES:
		DECIDUOUS TREES
	10	QUERCUS COCCINEA (SCARLET OAK) 10'-12' HT., B&B, FULL, BUSHY *40' DIAMETER MATURE CANOPY (350 SF PER CITY OF SEATTLE GREEN FACTOR PLANT LIST)
	10	CERCIDIPHYLLUM JAPONICUM (KATSURA) 8'-10' HT., B&B, FULL, BUSHY *40' DIAMETER MATURE CANOPY (350 SF PER CITY OF SEATTLE GREEN FACTOR PLANT LIST)
	22	ACER RUBRUM 'OCTOBER GLORY' (RED MAPLE) 14'-16' HT., B&B, FULL, BUSHY *40' DIAMETER MATURE CANOPY
	18	ZELKOVA SERRATA (JAPANESE ZELKOVA) 8'-10' HT., B&B, FULL *38' DIAMETER MATURE CANOPY (350 SF PER CITY OF SEATTLE GREEN FACTOR PLANT LIST)
	5	BETULA PAPYRIFERA (PAPER BIRCH) 8'-10' HT., B&B, FULL, BUSHY *35' DIAMETER MATURE CANOPY (250 SF PER CITY OF SEATTLE GREEN FACTOR PLANT LIST)
	11	POPULUS TREMULOIDES (QUAKING ASPEN) 16'-18' HT., B&B, FULL, BUSHY *30' DIAMETER MATURE CANOPY (250 SF PER CITY OF SEATTLE GREEN FACTOR PLANT LIST)
	26	ACER CIRCINATUM (VINE MAPLE) (3) STEMS MIN AT 1" CAL. EACH, B&B, FULL *25' DIAMETER MATURE CANOPY (75 SF PER CITY OF SEATTLE GREEN FACTOR PLANT LIST)
	49	AMELANCHIER x GRANDIFLORA (SERVICEBERRY) 6'-8' HT., B&B, FULL *15' DIAMETER MATURE CANOPY (75 SF PER CITY OF SEATTLE GREEN FACTOR PLANT LIST)
	35	CORNUS NUTTALLII (PACIFIC DOGWOOD) 6'-8' HT., B&B, FULL *20' DIAMETER MATURE CANOPY
		EVERGREEN TREES
	27	THUJA PLICATA (WESTERN RED CEDAR) 8'-10' HT., B&B, FULL, BUSHY *40' DIAMETER MATURE CANOPY 350 SF PER CITY OF SEATTLE GREEN FACTOR PLANT LIST)
	24	PSEUDOTSUGA MENZIESII (DOUGLAS FIR) 8'-10' HT., B&B, FULL, BUSHY *35' DIAMETER MATURE CANOPY (350 SF PER CITY OF SEATTLE GREEN FACTOR PLANT LIST)
	19	TSUGA HETEROPHYLLA (WESTERN HEMLOCK) 8'-10' HT., B&B, FULL, BUSHY *30' DIAMETER MATURE CANOPY (350 SF PER CITY OF SEATTLE GREEN FACTOR PLANT LIST)
	3	PINUS CONTORTA 'CONTORTA' (SHORE PINE) 10' HT., B&B, FULL, BUSHY *20'-35' DIAMETER MATURE CANOPY
	1	CALOCEDRUS DECURRENS (INCENSE CEDAR) 10'-12' HT., B&B, FULL, BUSHY *15' DIAMETER MATURE CANOPY

Current Design

Lighting Plan

SOFFITED ENTRIES AND GARAGE ENTRIES
Recessed Downlight
Flush-Mounted in Fiber Cement Soffits

RESIDENTIAL ENTRIES & Decks
Black cylinder light. Mounted Next to Exterior Doors

PATH LIGHTING
Entry Courts
Common Amenity Space Lighting

LANDSCAPE LIGHTING
Uplighting at landscape feature locations

12' LIGHT POLE
Driveway & security lighting

Current Design

Community Concept

A clean, contemporary aesthetic with gabled roofs that is in keeping with the existing neighborhood trend and was encouraged by both the neighbors and Design Review Board at the Early Design Guidance meeting. Shed roof additions were encouraged at the first Design Recommendation meeting. Light, bright colors were selected in an effort to brighten the site due to its location in a bowl below Lake City Way.

THEMES FOUND THROUGHOUT SITE:

A variety of color and cedar siding accents. Accent colors are scattered throughout the site to diminish repetition.

Decks at the 2nd level extend amenity space for residents and provide additional eyes on the street, at primary green spaces, and at entry courts.

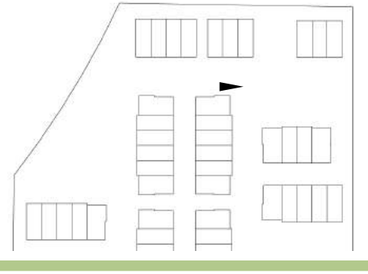
Juliette Balconies and additional windows were added to motor court facing elevations to treat the spaces more like "back patios."

Ground-level windows and building modulation were added and create more active side elevation conditions.

Vertical continuity of materials and wall/roof interaction helps to further define each individual unit.

Raised pedestrian crossings connect the various outdoor spaces and actively work to create a network of spaces throughout the site.

ENTRY PLAZA



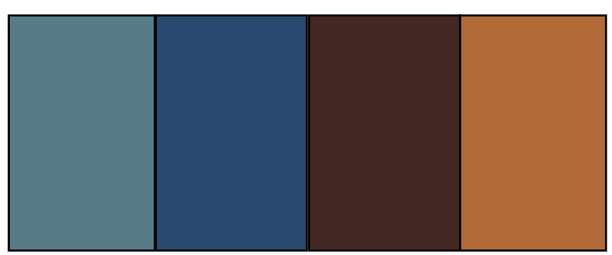
Current Design

At the first Design Review meeting the board suggested a stronger dialogue between the siding and roof form is needed. (DC2-B) The Board noted that the change in materials and color application is not easily understandable. Building modulation on the front elevations has solved this dilemma and shed roof elements help to delineate individual units.

The public representatives from the neighborhood voiced their support for the simplicity of the color palette at the first Design Review meeting. Thus, it was maintained. Variety between color and cedar applications on the different buildings was added and the plinth-like lap siding was eliminated at the base of each building.

Greenscreens have been eliminated from building facades. They are still proposed adjacent to perimeter retaining walls. Care and maintenance of these retaining walls will be the responsibility of the HOA.

FIBER CEMENT LAP & PANEL - MAIN BODY COLORS



FIBER CEMENT PANEL - ACCENT COLORS

CEDAR SIDING

VINYL WINDOWS/ DOORS - WHITE

BOLT-ON METAL DECK (BLACK)

METAL RAILING (HORIZONTAL)

COMPOSITE ROOFING

GREENSCREEN

Material and Color Palette

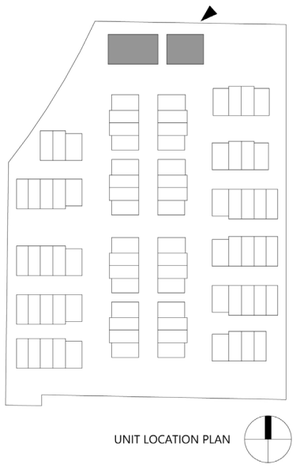
NOTE: KEY #'s CORRESPOND TO ELEVATIONS ON FOLLOWING PAGES

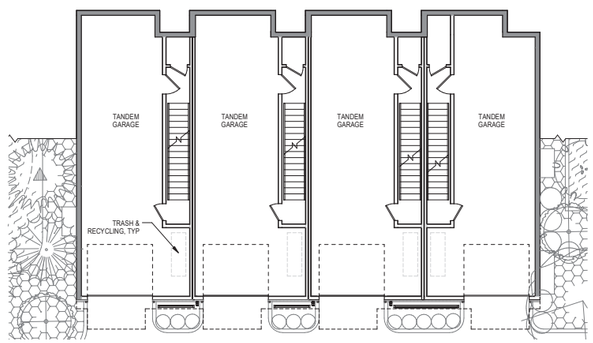
Elevations & Plans - Buildings A/B

The units along NE 88th St proved challenging from a grading perspective, as they are tied to the driveway grade along the south edge, and the new street improvement plan grade along NE 88th St. This resulted in a 2nd floor entry that is partly sunken below 88th at units A.1-B.1.We have used this opportunity to create more human-scaled buildings along NE 88th and are proposing a contemporary style with gabled roofs as a connection between the site and the existing neighborhood. These buildings utilize shed roof elements that align with the direction of the street grading, while still utilizing the more traditional gabled roof.

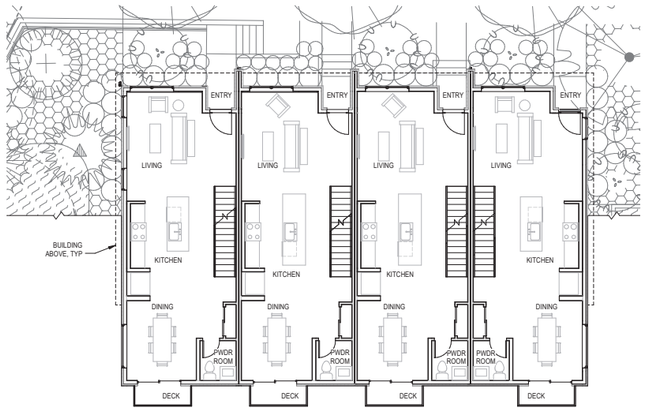
OLD FRONT ELEVATIONS PRESENTED AT DR1

NEW ENTRY CONDITIONS ALONG NE 88TH ST

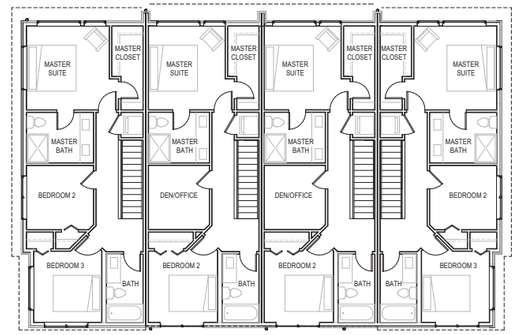




LEVEL 1



LEVEL 2



LEVEL 3

BUILDING A PLANS - BUILDING B SIMILAR (1 LESS UNIT)

NORTH (FRONT) ELEVATIONS - BUILDINGS A AND B - NE 88TH ST - TRANSITION FROM NEIGHBORHOOD TO SITE

NOTE: SEE PAGE 35 FOR MATERIAL INFORMATION

BUILDING B - WEST (SIDE) ELEVATION

BUILDING A - SOUTH (REAR) ELEVATION

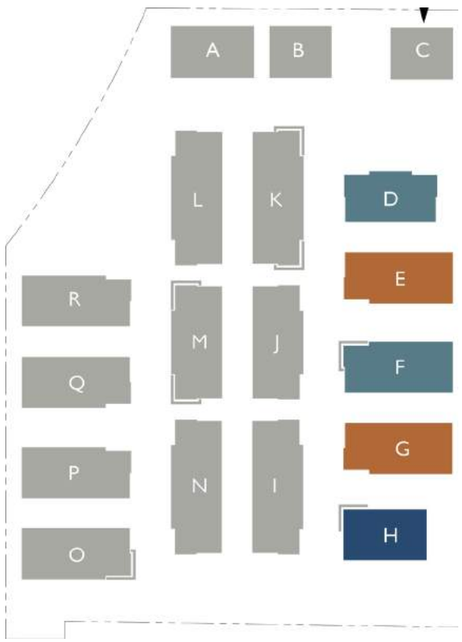
BUILDING B - SOUTH (REAR) ELEVATION

BUILDING A - EAST (SIDE) ELEVATION

Elevations & Plans - Building C

Unlike Buildings A and B, Building C appears as a 3-story building on NE 88th St. The design of this building is unique on the entire street. The openness of the corner unit at Levels 1 and 2, turns the corner from the facade facing NE 88th Street to the facade facing the drive aisle and entry sidewalk, helping to welcome people into the community.

All six buildings along the property line utilize the full shed roof form (buildings C-H). Color accents are incorporated at buildings D-H. The shed roof in this instance reduces the impact of these buildings to the neighbors over their previous design. The low side of the shed faces the East property line, which results in a better massing relationship with the neighbors.



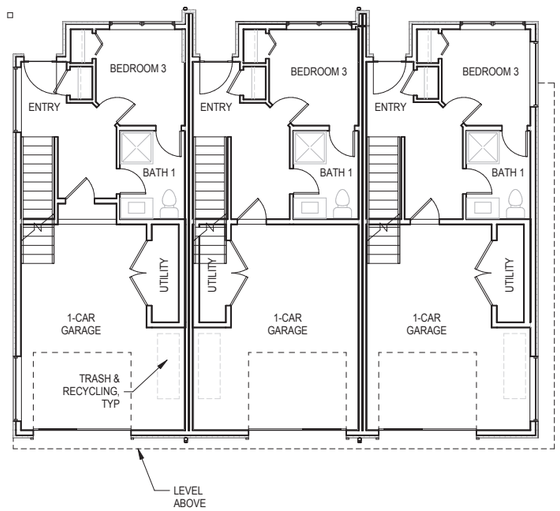
NEW NORTH (FRONT) ELEVATION - BUILDING C

OLD FRONT ELEVATION PRESENTED AT DR1

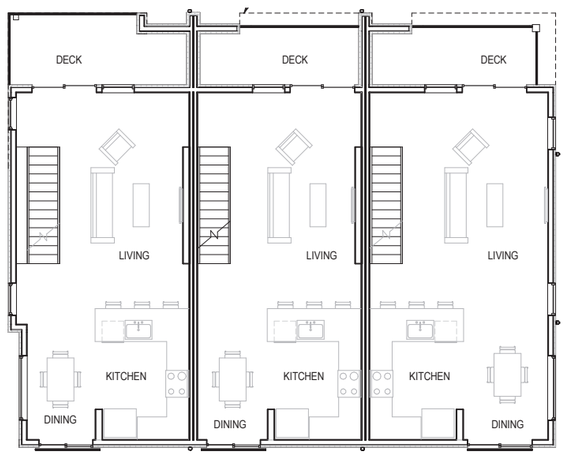
SOUTH (BACK) ELEVATION

SIDE (EAST) ELEVATION - ALONG PROPERTY LINE

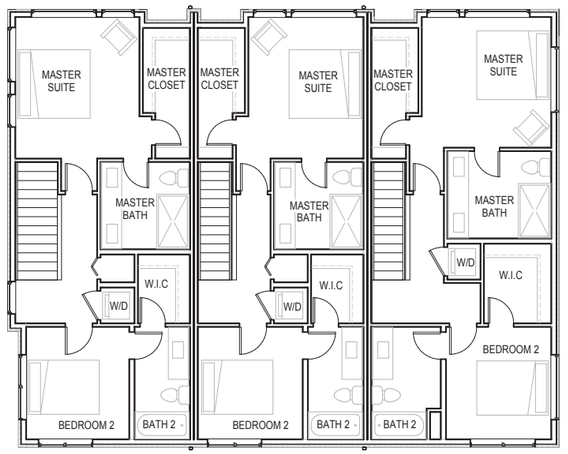
SIDE (WEST) ELEVATION - PUBLIC EDGE - ADJACENT SIDEWALK



LEVEL 1



LEVEL 2



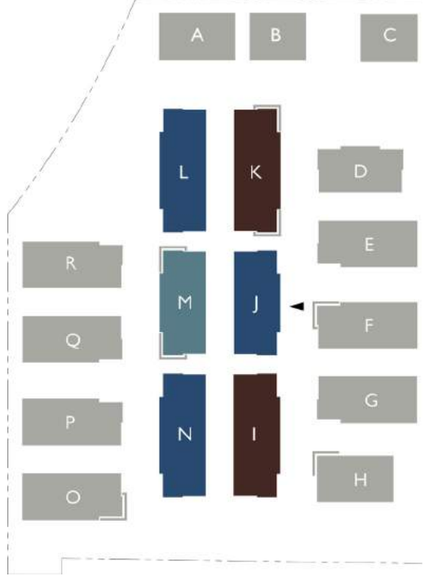
LEVEL 3

Elevations & Plans - Typical Alley-Loaded Building

The alley-loaded buildings utilize a gabled roof with shed roof elements to help identify individual units. The gable was maintained specifically for these buildings as it was determined that a full shed roof at each unit was visually cluttered upon entering the site at the driveway looking South.

The end units both address the corners with 3rd floor building modulation and color or cedar accents in addition to the second level deck that wraps the corner.

NEW FRONT ELEVATION - BUILDING J

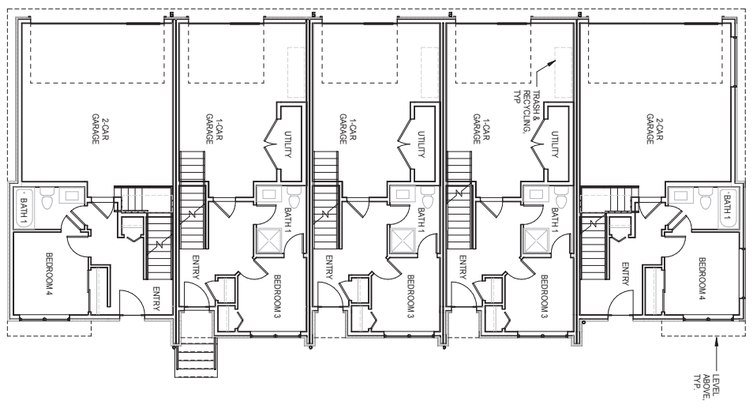


OLD FRONT ELEVATION PRESENTED AT DR1

BACK ELEVATION

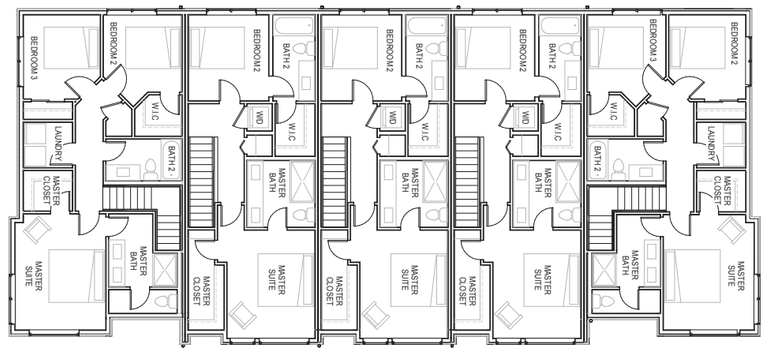
SIDE ELEVATION - PUBLIC EDGE -
ADJACENT SIDEWALK

SIDE ELEVATION - PRIVATE EDGE



LEVEL 1

LEVEL 2



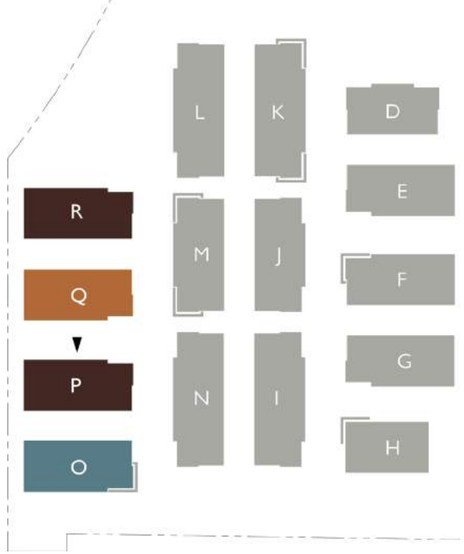
LEVEL 3

Elevations & Plans - Building P

The 4 buildings along the West Property Line kept the gabled roof with shed elements that is used for the Alley-loaded buildings. A full shed roof to match the buildings along the east property line was evaluated. However, since these buildings will be among the most visible in the community we're proposing the gable condition that the public voiced their preference for.

Cedar and color accents relate better to the roof design, and shed elements help to distinguish individual units.

NEW FRONT ELEVATION - BUILDING P

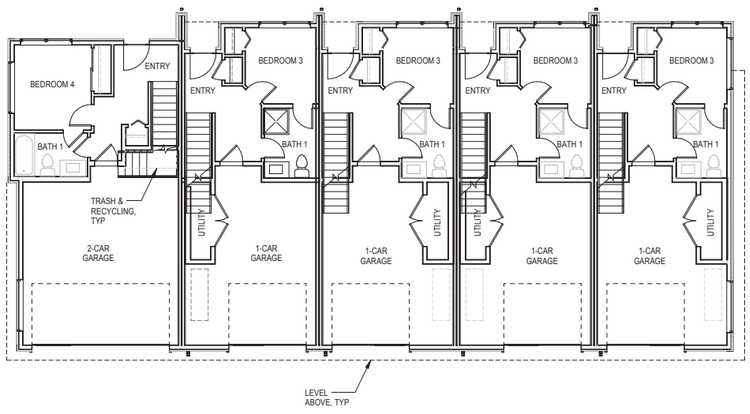


OLD FRONT ELEVATION PRESENTED AT DR1

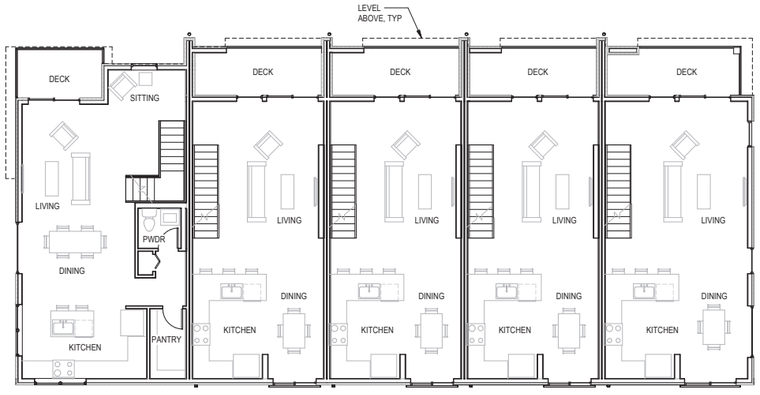
SIDE ELEVATION - PROPERTY LINE

BACK ELEVATION

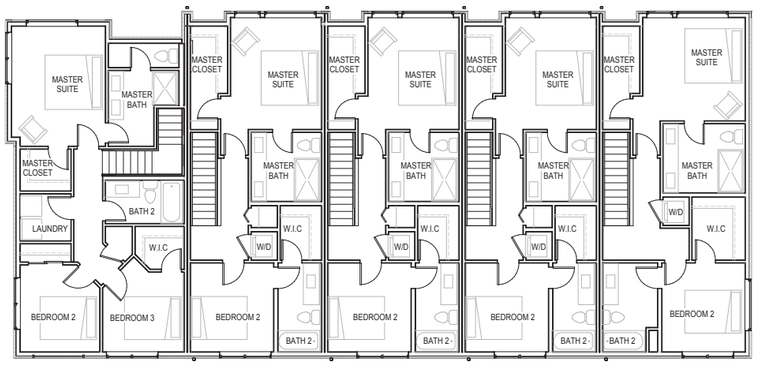
SIDE ELEVATION - FACING DRIVE AISLE



LEVEL 1



LEVEL 2



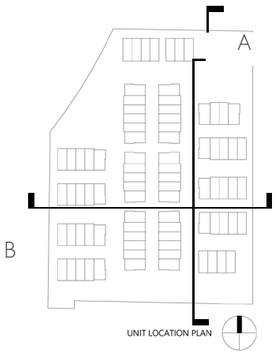
LEVEL 3

Current Design

Site Sections

A. SITE SECTION NORTH TO SOUTH

B. SITE SECTION EAST TO WEST



Perimeter Elevations

1 NORTH PERIMETER ELEVATION

2 EAST PERIMETER ELEVATION

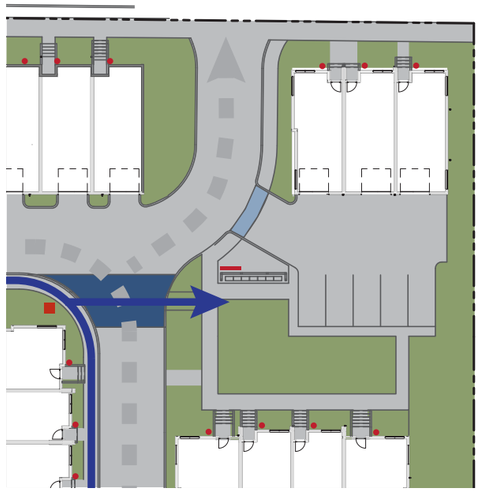
WOOD FENCING SHOWN IN ORANGE/ BROWN

3 SOUTH PERIMETER ELEVATION

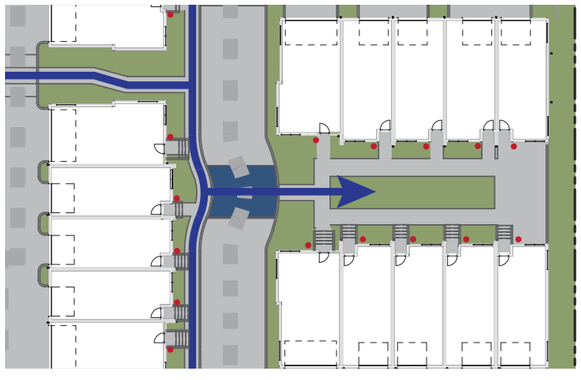
4 WEST PERIMETER ELEVATION

Current Design

Signage & Colors



- TRAFFIC WAYFINDING SIGN
- HOUSE NUMBER
- WAYFINDING SIGNAGE
- PRIMARY PEDESTRIAN CROSSING
- SECONDARY PEDESTRIAN CROSSING
- VEHICULAR TRAFFIC
- RAISED PEDESTRIAN CROSSWALK
- CONCRETE CROSSWALK

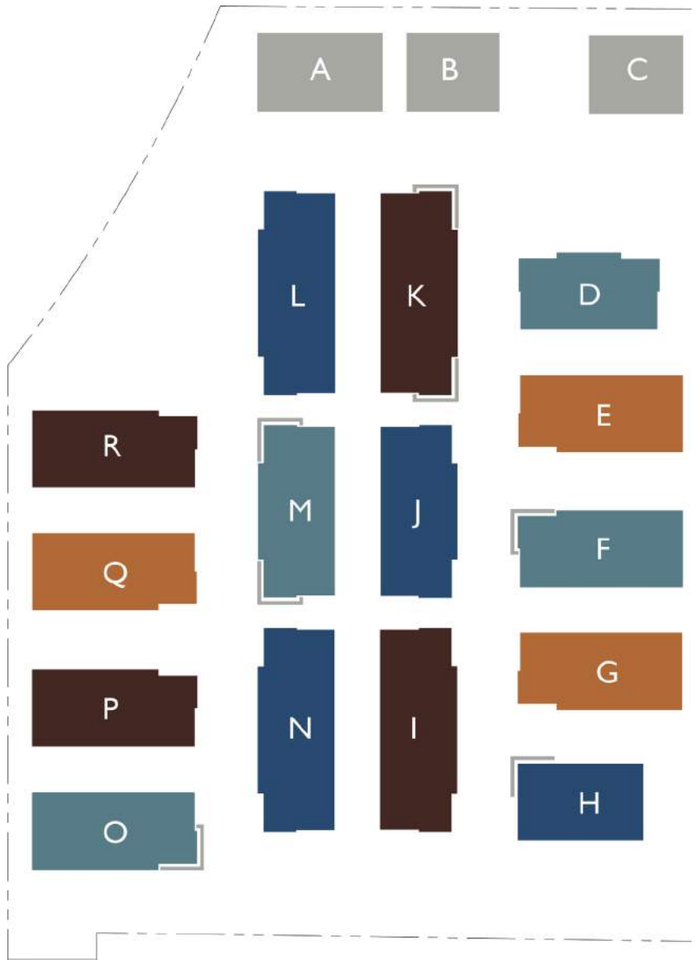


Signage Diagrams

At entry court locations, center units have been mirrored from the center units in the building across the entry court in order to reduce direct sightlines between neighbors. End units that directly face each other address direct sightlines through vegetation to better promote resident privacy.

House Numbers

Signage: Matte black 8" house numbers will be mounted on front elevations adjacent front doors. Black was selected over brushed aluminum for better contrast against the white panel siding.



Site Accent Color Key

Color Concept

Throughout the site a neutral palette is proposed for the main body colors of each building. Buildings along NE 88th St. help to softly transition the material palette into our development site with neutral colors and cedar siding accents. Once on site, the neutral palette and cedar accents are maintained with the addition of an accent color that changes between buildings to help provide a sense of individuality. The accent colors, while repeated, are scattered throughout the site in an effort to avoid repetition. Additionally, where corners are highlighted in gray, the cedar will be utilized at the 3rd floor building modulation opposed to the accent color, adding additional variety to the siding treatment of each building.

Appendix

Landscape

Landscape Plan & Palette

WEISMAN**DESIGN**GROUP

1 Custom Benches

2 Common Area Table and Chairs

3 Permeable Concrete

4 Greenscreen (at retaining walls only)

5 At-grade bioretention and pathway

6 Wood Guardrail / Fencing

7 Natural Play - Log Tunnel

8 Natural Play - Climbing Rock & Log Steppers


Acer circinatum


Acer circinatum
fall


Amelanchier x grandiflora
'Autumn Brilliance'


Amelanchier x grand. 'Autumn
Brilliance' - fall


Mahonia repens


Physocarpus capitatus


Cornus sericea 'Kelseyii'


Cornus stolonifera - winter


Mahonia aquifolium


Gaultheria shallon


Cornus sericea 'Kelseyii'
winter


Cornus stolonifera


Calocedrus decurrens


Pseudotsuga menziesii


Thuja plicata


Quercus coccinea
fall


Quercus coccinea


Cercidiphyllum Japonicum
fall


Cercidiphyllum japonicum


Taxus x media 'Hicksii'


Carex obnupta


Achillea millefolium


Polystichum munitum


Arctostaphylos uva-ursi


Tiarella trifoliata


Vaccinium ovatum


Symphioricarpos albus

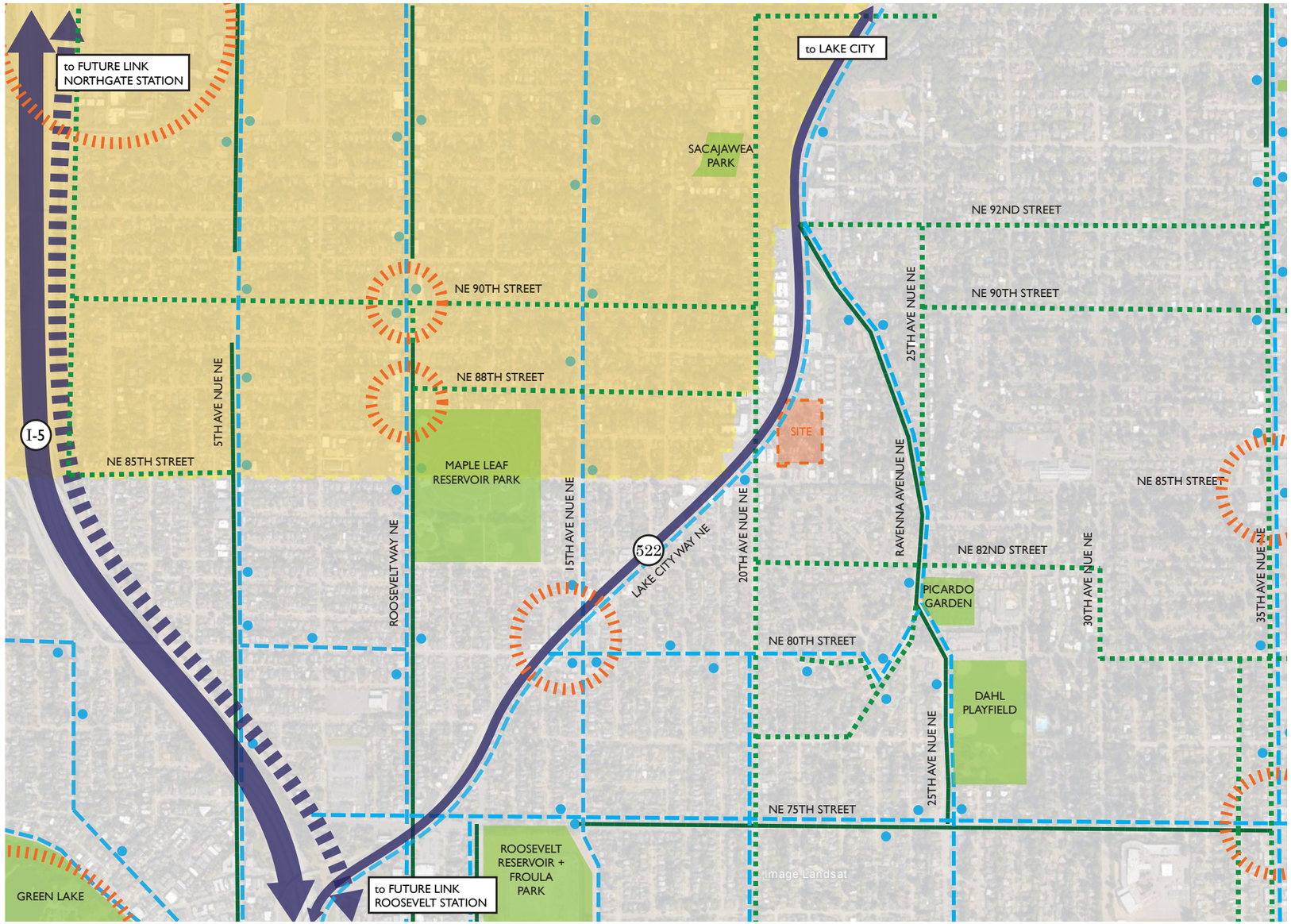
Appendix

TRANSIT

The project site is directly served by King County Metro routes 309 and 312 at the Lake City Way and NE 85th St. bus stop located .2 miles from the site. These two routes do not combine to provide for frequent transit service to the site. Additionally, Route 72, located at Ravenna Ave NE and NE 90th St, is .3 miles from the site. These 3 routes provide service from Lake City north to Bothell/ Kenmore area, and south to the University District and Downtown.

CYCLING

The project site is not directly served by roads with dedicated bike lanes. However, it is decently located for cycling opportunities at Maple Leaf Reservoir Park, Green Lake, and the Burke Gilman Trail is 1.8 miles to the east, best accessed via NE 95th St.



MAP KEY

- SITE
- PARK
- NORTHGATE OVERLAY DISTRICT
- NODE
- BUS ROUTE
- BUS STOP
- BIKE DEDICATED LANES
- BIKE FRIENDLY LANES
- INTERSTATE/ STATE ROUTE
- FUTURE LIGHT RAIL

Zoning

SITE

-Zoned LR2 with a small portion of C1-65 in the South West corner of the property
-Parcels to the North and West zoned C1-65/40
-Parcels to the East and South zoned LR1
-Transition zone between high density C1 lots adjacent Lake City Way and lower density LR and SF zoning to the SW.

NEIGHBORHOOD DEVELOPMENT:

Located on the east side of Lake City Way where it curves to the north, and at the boundary between Ravenna and Northgate, this lowrise residential site is surrounded by a mix of commercial and multi-family zones, transitioning to single family a block away to the east and south. Although some properties are older or have not been maintained to the highest conditions (including the existing mobile home park on the development site), new development in the form of several townhouse sites and a new apartment building to the west are bringing change to the area.

Abutting the southwest side of the site is a small vacant lot just north of a larger parcel containing the aforementioned new apartment building. This building is five stories and is overlooking the project site. To the south and east of the project are a mix of mostly small apartment buildings and townhouses, all facing away from the site towards their respective streets.

Across Lake City Way to the west is a gas station which is seasonally screened by street trees within the right-of-way and on the project site. Mid-size apartment buildings are located at the intersections on either side of the gas station. Further south along Lake City Way, there is a mix of retail and commercial buildings providing restaurants, three auto repair shops, a motorcycle dealership, a convenience store, and several small office and retail centers. Bus service to the site is provided by a stop located about a block south, and is served by routes 309 and 312.

To the north is a used car lot and a pair of smaller apartment buildings, then a series of retail buildings facing Lake City Way that include private storage, two more auto repair shops, a Thai restaurant, assisted living facility, and other small office and retail spaces. Wedgewood Elementary School is three blocks to the east.

Appendix

A. Site Information & EDG Selection

Option 3 - Pedestrian Oriented & Preferred

DISTINGUISHING FEATURES - EDG OPTION

Total Units: 89 (16' x 38', 16' x 40', and 20' x 36')
Resident Parking Stalls: 127 (36 two-car garages & 55 1-car garages)
Guest Parking Stalls: 27
Average Unit Size (includes garage): 1,943 Sq Ft
Total Gross Floor Area: 176,832 Sq Ft
FAR Net Floor Area: 162,685 Sq Ft
FAR achieved: 162,685 / 157,639 = 1.03
Target FAR: 1.20

PROS

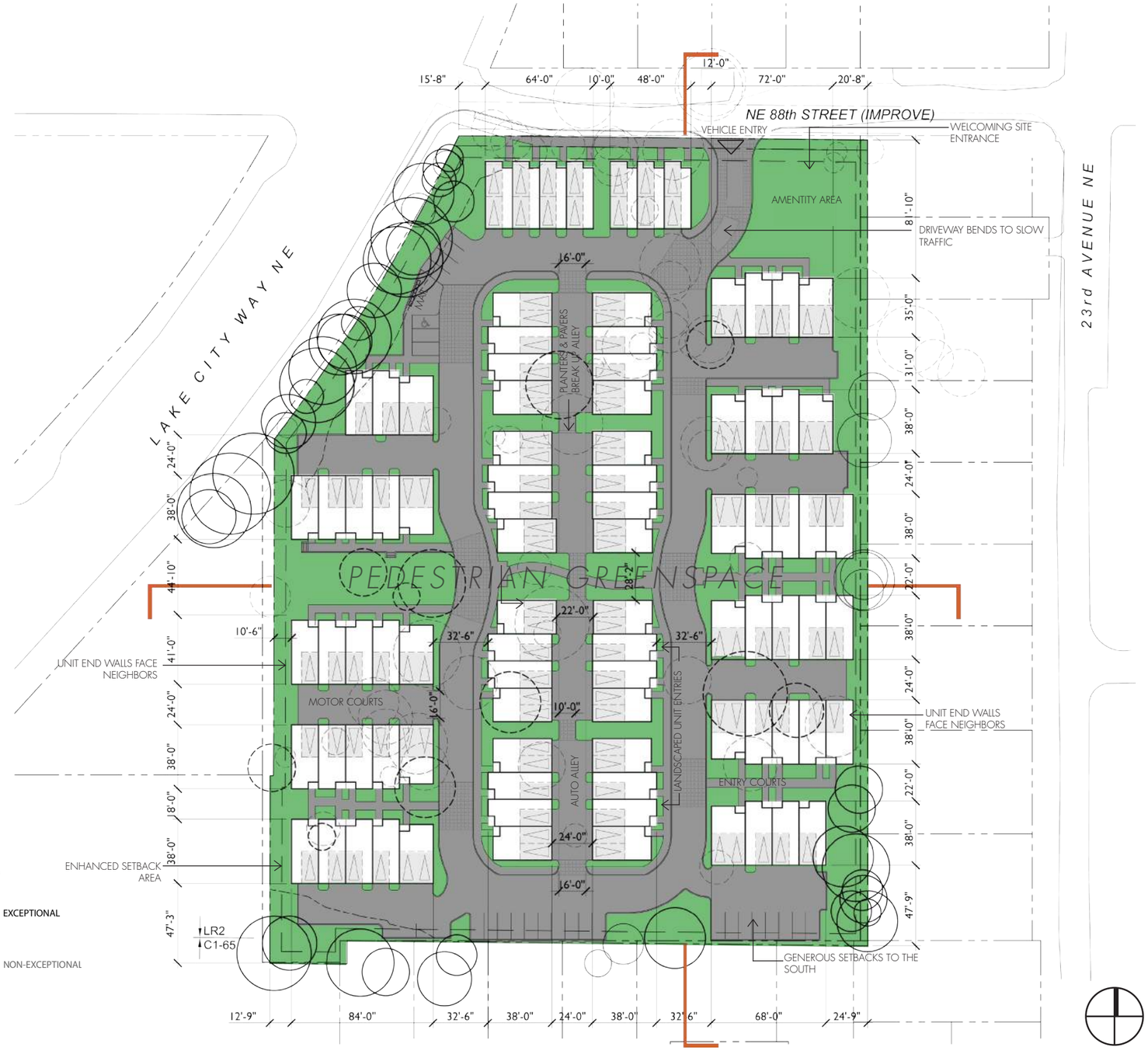
-Central Amenity Space featuring significant landscaping and gathering space
-Additional guest parking relieves side streets of overflow parking
-Additional unit types offer variety of options for future buyers
-Hidden garage doors in central alley and motor courts
-Meandering roads and center aisle increase pedestrian safety
-Instead of garage doors, residential entries & unit end-walls face the driveway.
-Majority of unit fronts have greater planting space
-Greater privacy for neighboring properties
-Community oriented space at entry of the site

CONS

-Greater amount of paved surfaces due to increased guest parking.

CURRENT DESIGN - UPDATES TO EDG OPTION

-The target FAR was reduced to 1.0, and the proposed FAR is now 0.99. This allows us to maintain our driveway location and proposed guest parking adjacent the mail area.



CODE ANALYSIS

PARCEL #: 510140-0770

ZONING: LR2, C1-65

OVERLAYS: none

LOT AREA: 157,639 Sq Ft (or 3.62 Acres)

ECA: Steep Slope, Liquefaction

PERMITTED USES (23.45.504)
Permitted Outright: Residential

FLOOR AREA RATIO (23.45.510):

LR2		C1-65	
Townhouses:	1.0 or 1.2*	Single Use:	4.25

DENSITY LIMIT (23.45.512):
Townhouses: 1/1600 or no limit*

* In LR zones, in order to qualify for the higher FAR & density limit shown, green building performance and other site access and parking standards shall be met

STRUCTURE HEIGHT (23.45.514):
30' base height limit
+5' for roof w/ minimum 6:12 pitch
+3' for shed or butterfly roofs
+4' for apartments w/a story that is partially below grade (caveats)
+10' for stair or mechanical penthouses (limited to 20% roof coverage if mech is on roof)
Roofs enclosed by a parapet may exceed the height limit by 75% provided the lowest point of the roof is at or below the height limit.

SETBACKS (23.45.518):

Front:	5' min + 7' Avg
Rear:	7' avg + 5' min
Side (Up to 40' façade):	5' min
Side (Over 40' façade):	7' avg + 5' min

AMENITY AREA (23.45.522):
25% of the lot area = 39,410 SF required amenity area

GREEN FACTOR (23.45.524):
Landscaping that achieves a Green Factor score of 0.6 or greater

STRUCTURE WIDTH (23.45.527):
Townhouses: 90' max
Max side façade length: 65% of side lot line for portions within 15' of lot line

AUTOMOBILE PARKING (23.45.536):
1 stall per unit

BICYCLE PARKING (23.54.015) Table D:
1 long-term stall per every 4 units

SOLID WASTE (23.54.040):
Shared collection (min horiz dimension = 12'):

9-15 residential units	150sf
16-25 dwelling units	225sf
26-50 dwelling units	375sf
51-100 dwelling units	375sf + 4sf for each unit above 50
101+ dwelling units	575sf + 4sf for each unit above 100

Individual collection:
2' x 6' storage area for each dwelling unit located on the same lot

C. NK Project Examples

FOURTH + ROY TOWNHOMES

HARBOR LIVE/WORK

17TH CAPITOL HILL TOWNHOMES

CREEKSIDE TOWNHOMES

15TH BALLARD LIVE/WORK

GALER 8 TOWNHOMES

ADMIRAL LOFT TOWNHOMES

WOODLAND PARK TOWNHOMES

SALVEO TOWNHOMES

CENTERRA

SPRING PEAK TOWNHOMES

PERSPECTIVE AT TERRAINE & WEST JULIAN STREET

RALLY TOWNHOMES

ELEMENT

COPPERRIDGE